

City of
IRWINDALE
AGENDA REPORT

Date: February 26, 2025
To: Honorable Mayor and Members of the City Council
From: Julian A. Miranda, City Manager
Issue: Irwindale Gateway Specific Plan

City Manager's Recommendation:

That the City Council consider the Planning Commission's Resolutions Nos. 834(24), 835(24), 836(24), 837(24), and 838(24) recommending the City Council certify the FEIR and approve the proposed Irwindale Gateway Specific Plan and associated entitlements; and:

1. **Adopt Resolution No. 2025-01-3589**, entitled "A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IRWINDALE (1) CERTIFYING THE ENVIRONMENTAL IMPACT REPORT, STATE CLEARING HOUSE (SCH# 2023020290); (2) ADOPTING THE FINDINGS REQUIRED BY CEQA GUIDELINES, SECTION 15091; (3) ADOPTING THE PROPOSED MITIGATION MONITORING AND REPORTING PROGRAM; AND (4) ADOPTING A STATEMENT OF OVERRIDING CONSIDERATIONS, PURSUANT TO THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) FOR THE IRWINDALE GATEWAY SPECIFIC PLAN";
2. **Adopt Resolution No. 2025-02-3590**, entitled "A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IRWINDALE AMENDING THE COMMUNITY DEVELOPMENT ELEMENT AND LAND USE PLAN MAP OF THE CITY'S GENERAL PLAN (GPA NO. 02-2022) FROM REGIONAL COMMERCIAL TO SPECIFIC PLAN FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNs: 8532-002-046 AND 8532-002-047) AS SET FORTH HEREIN AND MAKING FINDINGS IN SUPPORT THEREOF";
3. **Introduce Ordinance No. 787**, entitled "AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF IRWINDALE, CALIFORNIA, ADOPTING ZONE ORDINANCE AMENDMENT NO. 02-2023 TO ADD IRWINDALE GATEWAY SPECIFIC PLAN TO TITLE 17 OF THE IRWINDALE MUNICIPAL CODE (IMC), DEFINING PERMITTED USES, REGULATING THE SIZES AND LOCATIONS OF BUILDINGS, SPECIFYING DESIGN GUIDELINES AND DEVELOPMENT STANDARDS, ADDRESSING MOBILITY AND CONNECTIVITY TOPICS, IDENTIFYING PROJECT INFRASTRUCTURE AND SERVICES, AND PROVIDING ADMINISTRATIVE AND IMPLEMENTATION LANGUAGE FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-047) AND FINDING THE PROJECT CONSISTENT WITH THE CERTIFIED FINAL ENVIRONMENTAL IMPACT REPORT (SCH# 2023020290)";
4. **Introduce Ordinance No. 788**, entitled "AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF IRWINDALE MODIFYING THE CITY OF IRWINDALE ZONING MAP (ZONE CHANGE NO. 02-2022) FROM M-2 (HEAVY MANUFACTURING) TO IRWINDALE GATEWAY SPECIFIC PLAN FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-047) AND FINDING THE PROJECT CONSISTENT WITH THE CERTIFIED ENVIRONMENTAL IMPACT REPORT (SCH# 2023020290)";

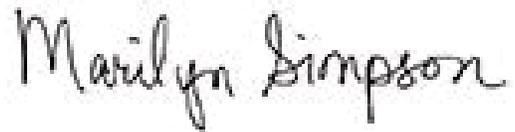
and

5. **Adopt Resolution No. 2025-05-3593**, entitled "A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IRWINDALE APPROVING TENTATIVE PARCEL MAP NO. 83854 TO CREATE SEVEN (7) PARCELS FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-04) IN THE M-2 (HEAVY MANUFACTURING) ZONE SUBJECT TO CONDITIONS AS SET FORTH HEREIN, MAKING FINDINGS IN SUPPORT THEREOF."

Administrative Action:

Submitted by:

Marilyn Simpson, AICP, Community Development Director



Prepared by:

Brandi Jones, Senior Planner



Reviewed by:

Adrian R. Guerra, City Attorney

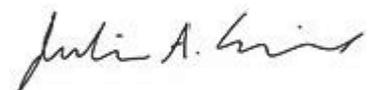


Kambiz Borhani, Finance Director / City Treasurer



Approved by:

Julian A. Miranda, City Manager



Background and Analysis:

REQUEST

On January 22, 2025, the City Council continued this item to the February 26, 2025 City Council meeting to be on the Consent Calendar. They directed Staff to revise the Irwindale Gateway Specific Plan document to remove all references to Battery Energy Storage Systems (BESS), and accompanying Resolutions and Ordinances.

A redlined version of the Specific Plan document has been provided which shows a red ~~strikethrough~~ of deleted text and underline for new text. A clean version of the Specific Plan document has also been provided where all ~~strikethrough~~ and added text have been deleted. The clean version is an exhibit of Ordinance No. 787.

Fiscal Impact:

None

Attachments:

1. ATTACHMENT A: Redlined Irwindale Gateway Specific Plan (dated February 20, 2025)
2. ATTACHMENT B: Resolution No. 2025-01-3589 for FEIR, MMRP, FOF/SOC (SCH# 2023020290) (with Exhibits)
3. ATTACHMENT C: Resolution No. 2025-02-3590 for General Plan Amendment No. 02-2022 (with map)
4. ATTACHMENT D: Ordinance No. 787 for Zone Ordinance Amendment No. 02-2023 (with Clean Irwindale Gateway Specific Plan, dated February 20, 2025)
5. EXHIBIT A to Ordinance No. 787 Irwindale Gateway Specific Plan CC Clean 02202025
6. ATTACHMENT E: Ordinance No. 788 for Zone Change No. 02-2022 (with map)
7. ATTACHMENT F: Resolution No. 2025-05-3593 for Tentative Parcel Map No. 83854



IRWINDALE GATEWAY SPECIFIC PLAN

PREPARED BY
SAGECREST PLANNING+ENVIRONMENTAL



SAGECREST
PLANNING + ENVIRONMENTAL

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INTRODUCTION

I.

1. Introduction

1.1 Overview

The Irwindale Gateway Specific Plan (“Specific Plan”) provides a comprehensive land use program to guide the development of an approximately 66.64-acre property in the City of Irwindale, California. The Specific Plan covers an approximately 52.65-acre developable portion (the “Development Area”) and 13.99 acres of undevelopable area (SCE easement and roadway dedications). The Specific Plan land use concept features an industrial business park with up to 997,796 square feet of building space ~~and a number of utility-scale battery energy storage system facilities~~. The Development Area is envisioned as a place where industrial businesses can thrive and encourage further economic investment in the City of Irwindale (the “City”). Its contemporary design will enhance the visual quality of the currently vacant site.

Notwithstanding anything to the contrary contained herein, any improvements shown on the figures contained herein are conceptual and for illustrative purposes only, and it shall not be a requirement that improvements be located, designed, or constructed as shown on such figures.

1.2 Background

The Development Area is the site of a former quarry. In the late 1980’s, quarry activities ceased, and the site was designated for reclamation as required by the State Surface Mining and Reclamation Act of 1975 (SMARA). On December 15, 1994, the City adopted Resolution 94-55-1381, approving a conditional use permit authorizing the operation of an inert landfill to “fill and restore the depleted gravel pit.” In connection with the approval, the City prepared and certified an Environmental Impact Report, State Clearinghouse No. 88060819, and adopted a related Statement of Overriding Considerations. Additionally, there are three existing billboards on the site. An approved grading plan was issued on September 16, 2022 to fix previous inert material that had been improperly placed. The inert debris remedial grading operation is bringing the site back to productive use with geotechnically certified fill.

Figure 1-1 – Site Photo

1.3 Description of Specific Plan Contents

This Specific Plan guides development of the Development Area by setting forth a land use plan, transportation and circulation plan, infrastructure plans, development standards, and design guidelines that address building placement, architectural style, landscaping, and other design elements. The substantive design elements of the buildings create visual interest, unity, and compatibility through the treatment of exterior building materials, colors, and façades as well as building placement, massing, and scale. In addition, a cohesive landscaping program will provide pleasing views of the Development Area from on- and off-site locations. Altogether, the various elements of the built environment in the Development Area will enhance the visual quality of the site and create an attractive development within the City.

From a long-term operational perspective, the Specific Plan accommodates industrial, and business park, ~~and battery energy storage uses~~. These types of uses serve to encourage economic investment, jobs, and business opportunities in the City of Irwindale and assist in maintaining sustained economic stability and growth.

1.4 Description of Specific Plan Area

As shown on Figure 1-2, Vicinity Map, the Specific Plan area is located in the western portion of the City of Irwindale, immediately east of I-605, north of Live Oak Avenue and south of Arrow Highway. The City of Baldwin Park is located to the southeast. The I-605 Freeway forms the western boundary of the Specific Plan Area with the I-605/Live Oak Avenue interchange located immediately southwest of the Specific Plan Area and the I-605/Arrow Highway interchange located immediately northwest of the Specific Plan Area.

As shown on Figure 1-3, Aerial Photograph, surrounding land uses include the Southern California Edison Rio Hondo Sub-station, located on the south side of Live Oak Avenue, and the Santa Fe Dam Flood Control Basin, located on the north side of Arrow Highway. The land located east of the Specific Plan Area is used for industrial purposes. Access to the Specific Plan Area is provided through an established local roadway network. The Development Area is within walking distance (half mile) of the nearest Foothill Transit bus stop (Line 492) on Live Oak Avenue and Stewart Avenue.

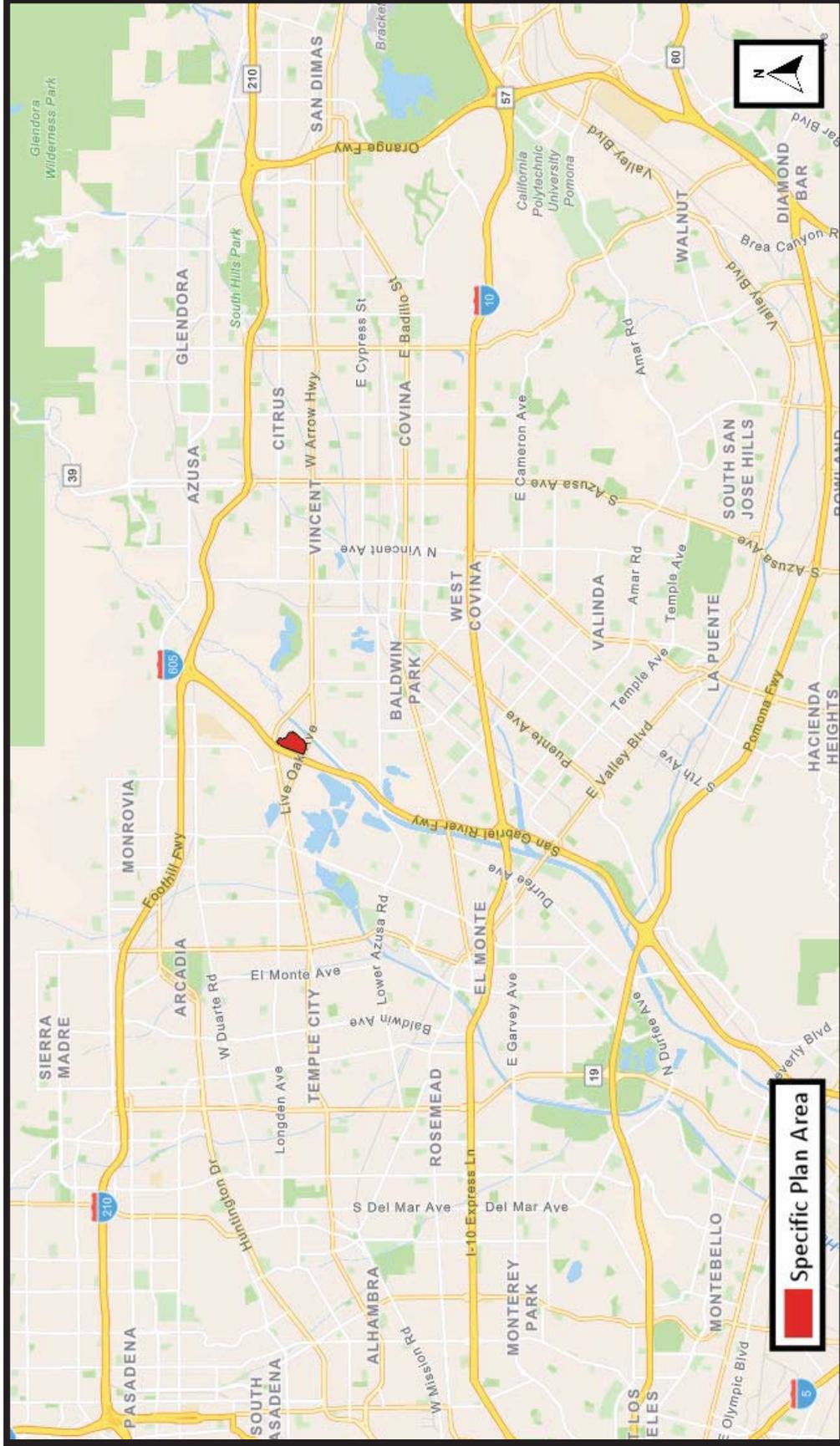


FIGURE 1-2 – VICINITY MAP

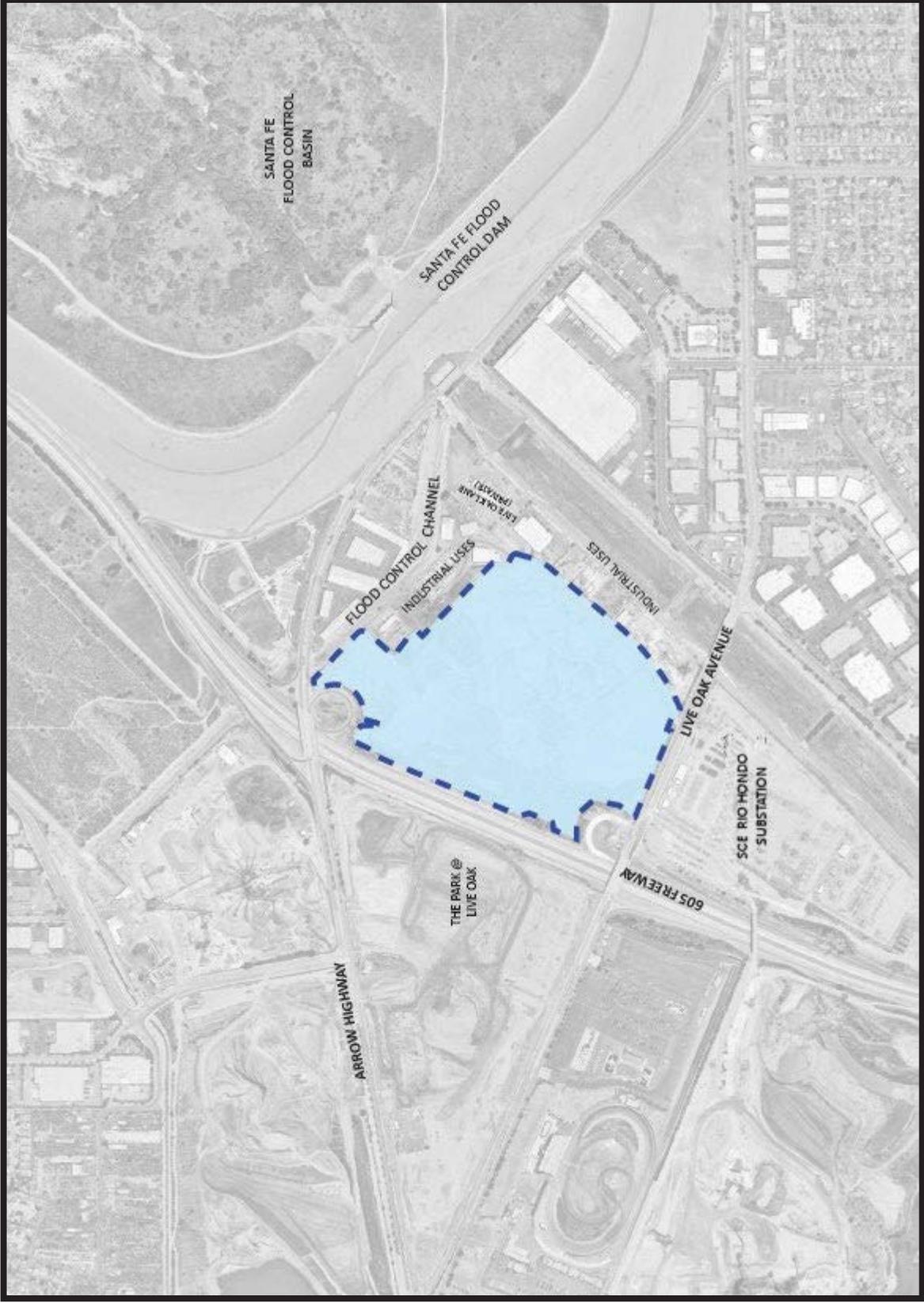


FIGURE 1-3 – AERIAL PHOTOGRAPH

1.5 Specific Plan Organization

This Specific Plan is organized into the following sections:

- Section 1 – Introduction
- Section 2 – Purpose and Intent of Specific Plan
- Section 3 – Relationship to Other Land Use Regulations
- Section 4 – Land Use
- Section 5 – Transportation and Circulation
- Section 6 – Development Standards
- Section 7 – Design Guidelines
- Section 8 – Utility Infrastructure Plan
- Section 9 – Implementation

Unless otherwise defined in this Specific Plan, the meaning of capitalized words, phrases, titles, and terms shall be the same as provided in the City of Irwindale Municipal Code and the City of Irwindale Commercial and Industrial Design Guidelines.

Purpose and Intent of Specific Plan

II.

2. Purpose and Intent of Specific Plan

2.1 Purpose and Intent

This Specific Plan, adopted by ordinance, establishes the development regulations, allowable land uses, design guidelines, and implementation procedures for the Development Area. The Specific Plan establishes the effective zoning for the Development Area. In addition, circulation and other infrastructure improvements such as water, wastewater, and stormwater drainage systems are addressed by this Specific Plan to ensure their proper sizing and timely installation.

2.2 Specific Plan Authority

This Specific Plan is a regulatory document prepared pursuant to the provisions of California Government Code §§ 65450 through 65457, which grant local government agencies the authority to prepare specific plans for the systematic implementation of their general plan for all or part of the area covered by the general plan.

California Government Code §§ 65450 through 65457 establish the authority to adopt a specific plan, identify the required contents of a specific plan, and mandate consistency with the general plan.

According to California Government Code § 65451:

- (a) A specific plan shall include text and a diagram or diagrams which specify all of the following in detail:
 - (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
 - (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
 - (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
 - (4) A program of implementation measures including regulations, programs, public works projects, and financing measures, necessary to carry out items (1), (2), and (3)
- (b) The specific plan shall include a statement of the relationship of the specific plan to the general plan.

This Specific Plan includes each of the required elements listed above and establishes the essential link for how the Specific Plan is consistent with the policies of the City of Irwindale General Plan (the "General Plan"). The Specific Plan provides the City of Irwindale's staff and decision makers, as well as developers, investors, the community at-large, utility providers, public service districts, and others with a comprehensive plan establishing regulations, conditions, and programs for guiding the systematic development of the Development Area. All future development plans and implementing

actions within the Development Area are required to be consistent with the regulations set forth in this Specific Plan and with all other applicable City, County, State, and federal regulations.

2.3 Statement of Objectives

To ensure the functional integrity, economic viability, and positive aesthetic appearance of the Development Area, the following planning and development goals are established:

- Create a comprehensive land use plan for the re-use of a reclaimed sand and gravel quarry ~~including the development of utility scale battery energy storage facilities.~~
- Provide guidelines and standards for the development of state-of-the-art buildings that can accommodate various industrial and manufacturing uses, including warehouse distribution, logistics, and fulfillment centers with proximate access to Interstate 605 on- and off-ramps.
- Ensure that infrastructure plans for water, sewer, drainage, dry utility, and solid waste disposal are adequately designed for the Specific Plan.
- Provide a circulation system that meets transportation requirements and minimizes potential adverse impacts on the surrounding area.
- Provide guidelines and standards for architecture, landscaping, walls, fencing, lighting, and entry treatments that are compatible with the design and architecture of the surrounding uses.

Relationship to other Land Use Regulations

III.

3. Relationship to Other Land Use Regulations

California Government Code § 65454 requires a specific plan to be consistent with the local General Plan. As part of the adoption of this Specific Plan, a General Plan Amendment changes the General Plan land use designation for the Specific Plan Area from “Regional Commercial” to “Specific Plan”, and a Zoning Ordinance Amendment changes the zoning designation for the Specific Plan Area from “M2 (Heavy Manufacturing)” to “Irwindale Gateway Specific Plan”. The adoption of this Specific Plan establishes the zoning regulations for the Specific Plan Area in order to implement the General Plan land use designation of “Specific Plan” for the Specific Plan Area.

The Specific Plan provides for a land use plan and development standards, which reflect the objectives in Section 2, governing development of the principal physical components of the Development Area, including, but not limited to: buildings, ~~battery energy storage systems~~, circulation and parking facilities, landscaping, open space, and utility improvements. The Specific Plan provides for the standards that establish the general type, parameters, and character of development with the goal of creating an integrated and unified environment that is compatible with its surrounding area. The Specific Plan also provides a plan for infrastructure improvements to serve the Development Area including the development of a new detention basin within the SCE Easement Area for collection of project storm water flows. The detention basin will replace an existing, unimproved natural retention basin located within the SCE Easement Area.

3.1 General Plan Consistency

The Specific Plan is designed to meet the goals established in the City’s General Plan by providing a framework for the future development of the Development Area. The Specific Plan is designed to be consistent with, and serves as an extension of, the City’s General Plan. The Specific Plan is consistent with the following General Plan Land Use Element policies and objectives:

GENERAL PLAN POLICY	SPECIFIC PLAN CONSISTENCY
COMMUNITY DEVELOPMENT ELEMENT (CDE)	
<i>Issue Area – Land Use Planning: The City of Irwindale is committed to the development of a comprehensive land use plan that will enhance the City’s livability and economic base for future generations.</i>	
CDE Policy 1: The City of Irwindale, through continued comprehensive land use planning, will strive to preserve the overall mix of land uses and development in the community.	The Specific Plan is consistent with the City’s goal of preserving the overall mix of land uses and development in the community by setting forth a land use plan and development regulations for how the reclaimed former quarry located in the Development Area will accommodate industrial and business park uses, including battery energy storage systems.

CDE Policy 2: The City of Irwindale will continue to plan for the transition of the quarries located within the City to other land uses.	The Development Area is a remediated former quarry. The Specific Plan allows the transition of the site to a productive and economically beneficial development for the City of Irwindale.
CDE Policy 3: The City of Irwindale will continue to ensure that the type, location, and intensity of all new development and intensified developments adhere to the requirements that are specified for their particular land use category in the General Plan.	The Specific Plan provides a development plan, infrastructure plans, development standards, and design guidelines that address building placement, architectural style, landscaping, and other design elements to address the specific characteristics of the site consistent with the General Plan land use designation of Specific Plan.
CDE Policy 5: The City of Irwindale will continue to promote comprehensive development consistent with the General Plan as opposed to piecemeal and incremental planning.	The Specific Plan implements policies set forth in the General Plan by providing a comprehensive plan and direction for the long-term development of the Development Area. The Specific Plan provides for implementation of uniform development regulations and design guidelines and addresses sustainable practices, to promote compatibility with surrounding areas. Adoption of the Specific Plan provides for the comprehensive development of the Development Area, thereby avoiding planning of piecemeal development.
<i>Issue Area – Economic Development: The City of Irwindale intends to continue its pursuit and promotion of economic development that will provide jobs and revenue for the community.</i>	
CDE Policy 10: The City of Irwindale will promote development that will benefit the community as a whole in terms of both jobs and revenue generation.	The Specific Plan allows for up to 997,796 square feet of industrial and business park uses on a former quarry site. Revenue benefits to the City of Irwindale may include but not be limited to increased property tax revenue and point-of-sale tax revenue. In addition, the Specific Plan will allow for development of uses leading to creation of jobs that can be filled by residents of the City and surrounding communities.
<i>Issue Area – Urban Design: The City of Irwindale will continue its efforts in improving the appearance of the community.</i>	
CDE Policy 12: The City of Irwindale will continue to promote quality design in the review and approval of commercial and industrial development through the application of the commercial and industrial design guidelines.	The Specific Plan promotes quality design by providing guidelines and standards for landscaping, architecture, walls, fencing, signage, lighting, and entry treatments addressing the specific character of the site and proposed uses to maintain compatibility with

	the design and architecture of the surrounding uses.
CDE Policy 14: The City of Irwindale will continue to promote property maintenance in all areas of the City.	The Specific Plan defines the entities responsible for maintenance of publicly and privately-owned improvements within the Specific Plan Area, including roadways and utility infrastructure (refer to Section 9.10 and Table 9-1). Compliance with the Specific Plan’s maintenance program ensures that all improvements within the Specific Plan Area are properly and perpetually maintained.
CDE Policy 16: The City of Irwindale will continue to work towards the development of streetscape, sign standards, and a Public Art Program.	The Design Guidelines for the Specific Plan (refer to Section 7) establish comprehensive streetscape design standards for the Irwindale Gateway Specific Plan’s frontage on Arrow Highway, Live Oak Avenue and Live Oak Lane promoting a welcoming visual environment for employees, visitors, and passersby. Signage will conform to City standards.
INFRASTRUCTURE ELEMENT (IE)	
<i>Issue Area – Maintenance of Service Standards: City of Irwindale will continue to maintain the highest levels of public service to respond to the existing and future demand for such services.</i>	
<p>IE Policy 1: The City will continue to support the efforts of the City of Irwindale Public Works Department in maintaining the highest service standards feasible.</p> <p>IE Policy 2: The City will continue to cooperate with those utility providers in the City to ensure that sufficient infrastructure capacity is available to meet current and future service demands.</p>	The Specific Plan requires improvements to be made to roadways and public utilities/infrastructure in conjunction with future development of the Development Area and as required by the City of Irwindale and applicable public service providers. Improvements are required to be made by the developers as necessary to serve the Development Area while maintaining adequate service levels for existing surrounding land uses. Plans for water, sewer, storm water, dry utilities, and solid waste disposal are provided in Section 8, Utility Infrastructure.
<i>Issue Area – Traffic and Circulation: The City of Irwindale will strive to improve safe and efficient circulation in the City.</i>	
<p>IE Policy 3: The City of Irwindale will continue to develop and enhance the existing streets and intersections in the City.</p> <p>IE Policy 4: The City of Irwindale will strive to ensure that all new development implements its “fair-share” of infrastructure</p>	The Specific Plan includes roadway and sidewalk improvement plans to facilitate efficient vehicular and non-vehicular transportation through and around the Development Area. With implementation of the circulation improvements identified in Section 5 of the Specific Plan, traffic impacts generated

<p>improvements to offset the potential adverse impacts associated with the additional traffic that will be generated by the new development.</p>	<p>by development in the Development Area will be offset to the City of Irwindale’s satisfaction. Furthermore, developers of land within the Development Area will be responsible for funding and/or construction of the required infrastructure improvements as set forth by the Specific Plan.</p>
<p>RESOURCE MANAGEMENT ELEMENT (RME)</p>	
<p><i>Issue Area – Natural Resources. The City of Irwindale will continue to cooperate in the maintenance and conservation of the area’s natural resources.</i></p>	
<p>RME Policy 1: The City of Irwindale will continue to work with the quarries and other regulatory agencies to facilitate their reclamation.</p> <p>RME Policy 3: The City of Irwindale will work with the quarry owners and/or operators and regulatory agencies to help facilitate their timely reclamation.</p>	<p>The Development Area is a former quarry site, which is the subject of an ongoing inert debris remedial grading operation being conducted to return the site to productive use. This Specific Plan provides guidelines and development standards for the redevelopment of the site. As such, the Specific Plan provides a comprehensive plan for the transition of the site to a productive and economically beneficial development for the City of Irwindale.</p>
<p><i>Issue Area – Resource Preservation. The City of Irwindale will maintain and preserve those natural and man-made amenities that contribute to the City’s livability.</i></p>	
<p>RME Policy 11: The City of Irwindale supports the ethic of conservation of non-renewable resources. This includes efforts to reduce the use of energy (in any form), greenhouse gas (GHG) emissions (consistent with AB 32) and efforts to find new and more energy efficient methods for delivering services. The City supports the development of building standards that enable the community to design energy saving features such as solar energy systems, water efficient landscaping, and sustainable, green, and energy efficient building standards.</p>	<p>The Specific Plan allows industrial land uses adjacent to Interstate 605 and approximately 1.8 miles from Interstate 210. By its location near major transportation corridors, the Specific Plan has the potential to reduce vehicle miles traveled, which would reduce tailpipe emissions – a major source of greenhouse gases. In addition, buildings in the Development Area will be required to comply with the California Green Building Standards Code, the City’s Model Water Efficient Landscape Ordinance (MWELO), and will incorporate additional sustainable design features that minimize water use and maximize energy efficiency. The Specific Plan also includes allowance for incorporation of one or more BESS (as hereinafter defined) facilities, thereby providing opportunity for high value infrastructure that would allow for more use of sustainable renewable energy resources that will help to reduce GHG emissions as conventional energy generation sources are</p>

	phased-out.
<p><i>Issue Area – Mining and Reclamation. The following policies focus on those City policy actions that can be taken to improve environmental compliance, reclamation planning, and long-term economic improvement of the mines and quarries (inactive, active, and reclaimed) in Irwindale.</i></p>	
<p>RME Policy 19: The City of Irwindale will consider environmental justice issues as they are related to potential health impacts associated with air pollution and ensure that all land use decisions, including enforcement actions, are made in an equitable fashion to protect residents, regardless of age, culture, ethnicity, gender, race, socioeconomic status, or geographic location from the health effects of air pollution.</p>	<p>The Development Area is a former quarry that was filled improperly with inert debris and reclaimed beginning in 2023. Air pollution emissions historically associated with mining and filling have ceased and air pollutant emissions associated with development of the Specific Plan would be generated primarily by mobile sources (tailpipe emissions from vehicles traveling to and from the property). These air pollutant emissions are regulated at the federal and State levels through mandated vehicle engine performance standards and fuel content requirements. The Development Area is surrounded by flood control uses to the north and east, the Southern California Edison Rio Hondo sub-station to the south, and Interstate 605 and quarry uses to the west. As such, there are no known or probable environmental justice issues associated with developing the Specific Plan land uses.</p>
<p>RME Policy 25: The City of Irwindale will monitor traffic and congestion to determine when and where the City needs new transportation facilities to achieve increased mobility efficiency.</p>	<p>The Specific Plan requires roadway and sidewalk improvements for new developments to facilitate efficient vehicular and non-vehicular transportation through and around the Development Area. The Specific Plan provides for required roadway improvements, including to the north side of Live Oak Avenue and Live Oak Lane. Transportation and roadway improvements identified in Section 5 of this Specific Plan ensure that the segments of Live Oak Avenue and Live Oak Lane adjacent to the Development Area will operate at an acceptable service level.</p>
<p>PUBLIC SAFETY ELEMENT (PSE)</p>	
<p><i>Issue Area – Emergency Preparedness. The City of Irwindale will strive to maintain the highest levels of readiness to respond to disasters or local emergencies.</i></p>	
<p>PSE Policy 3: The City of Irwindale will work to reduce potential hazards through conscientious land use planning. The City shall require liquefaction assessment studies as</p>	<p>The Development Area is the site of a reclaimed former quarry. The inert debris deposited at the property during the reclamation process consists of a combination of crushed rock, fine</p>

<p>part of development proposals in areas identified by the California Geological Survey as susceptible to liquefaction. The studies shall be conducted in accordance with the California Geological Survey’s Special Publication 117; Guidelines for Evaluating and Mitigating Seismic Hazards in California, and the Southern California Earthquake Centers (1999) procedures to implement Special Publication 117 – Liquefaction Hazards (both documents are incorporated herein by reference). On sites shown to be susceptible to liquefaction, the City shall require the implementation of mitigation measures designed to reduce this hazard to an acceptable level. The City shall require a State certified engineering geologist or registered civil engineer; having competence in the field of seismic hazard evaluation and mitigation, to review the study at the Applicant’s expense. The review shall determine the adequacy of the hazard evaluation and proposed mitigation measures and determine whether the requirements of State law are satisfied, as described in Special Publication 117 by the California Geological Survey.</p>	<p>silt, and clean construction and demolition waste (e.g., broken concrete, asphalt, brick, soil) that complies with applicable State of California, Los Angeles County, and City of Irwindale standards. Prior to backfill, the inert debris was blended, moisture conditioned, and compacted for stability in accordance with State of California and City of Irwindale requirements of graded fill materials. Following completion of reclamation, the Development Area will be suitable for development and will not be subject to liquefaction or other geologic stability hazards. Additionally, site and development geotechnical investigations will be required for all subsequent development proposals within the Development Area. City approval of these investigations and incorporation of investigation requirements and recommendations will be required prior to the issuance of grading permits to ensure potential hazards due to liquefaction are addressed.</p>
<p><i>Issue Area – Noise: The City of Irwindale will work to reduce the high levels of noise exposure associated with the existing development and transportation facilities in the City.</i></p>	
<p>PSE Policy 5: The City of Irwindale will work towards reducing noise exposure in the City by considering noise and land use compatibility in land use planning.</p>	<p>The Development Area does not abut noise-sensitive land uses (e.g., residential, schools, etc.). The uses allowed within the Development Area are compatible with the surrounding industrial uses. Furthermore, the potential noise generated by future developments has been analyzed in the Irwindale Gateway Specific Plan EIR (as hereinafter defined) in accordance with the California Environmental Quality Act and potential impacts will be mitigated.</p>

3.2 Relationship to Zoning Code

The Title 17 (Zoning Code) of the Irwindale Municipal Code provides a citywide framework of regulations that addresses topics such as permitted uses, development standards, parking and landscaping regulations, permit procedures, and sign regulations.

The Specific Plan serves as the zoning for the Specific Plan Area, and as such, includes the development regulations applicable to the Specific Plan Area. In the event that any provision in the Specific Plan conflicts with the Zoning code, the provisions set forth within the Specific Plan shall prevail. In the event the Specific Plan is silent regarding any Zoning provision applicable to the Development Area, the Zoning Code shall prevail.

3.3 Subdivisions

All development in the Development Area shall comply with Title 16 (Subdivisions) of the Irwindale Municipal Code and the Subdivision Map Act (Cal. Government Code § 66410, et. seq), as applicable.

Land Use

IV.

4. Land Use

4.1 General Provisions

This section establishes allowable uses within the Development Area. The Specific Plan allows for a variety of uses, including professional offices, and industrial uses, ~~and battery energy storage.~~

The Development Area is planned for the development of warehousing, distribution, fulfillment center, industrial, manufacturing, ~~battery energy storage systems,~~ and office uses. The SCE Easement Area is planned for infrastructure improvements required to serve the Development Area. A breakdown of the size of the areas within the Specific Plan is as follows:

Table 4-1

Area	Size
Gross Site Area	66.64 acres
SCE Easement Area	9.61 acres
Right-of-Way Dedication	4.38 acres
Net Developable Area	52.65 acres

4.2 Summary of Land Use Plan

The Development Area may be developed using one of the two following land use plans:

Land Use Plan 1: Consisting of the entire 52.65-acre Development Area (Figure 4-1). If the Development Area is developed to Land Use Plan 1 the entire site would be developed with industrial, manufacturing, warehousing, distribution, fulfillment center, and office uses (collectively, "Industrial and Business Park Uses"). Sites which are developed with industrial, manufacturing, warehousing, distribution, fulfillment center, and/or office uses shall be referred to as "Industrial and Business Park Sites."

~~Land Use Plan 2: Consisting of using the southern 15.94 acres of the Development Area for one or more Battery Energy Storage System (BESS) facilities and the remainder 36.71 acres for industrial, manufacturing, warehousing, distribution, fulfillment center, and office uses (Figure 4-2).~~

The development of the Development Area according to one of the land uses plans shall not restrict redevelopment of the Development Area according to the other land use plan.

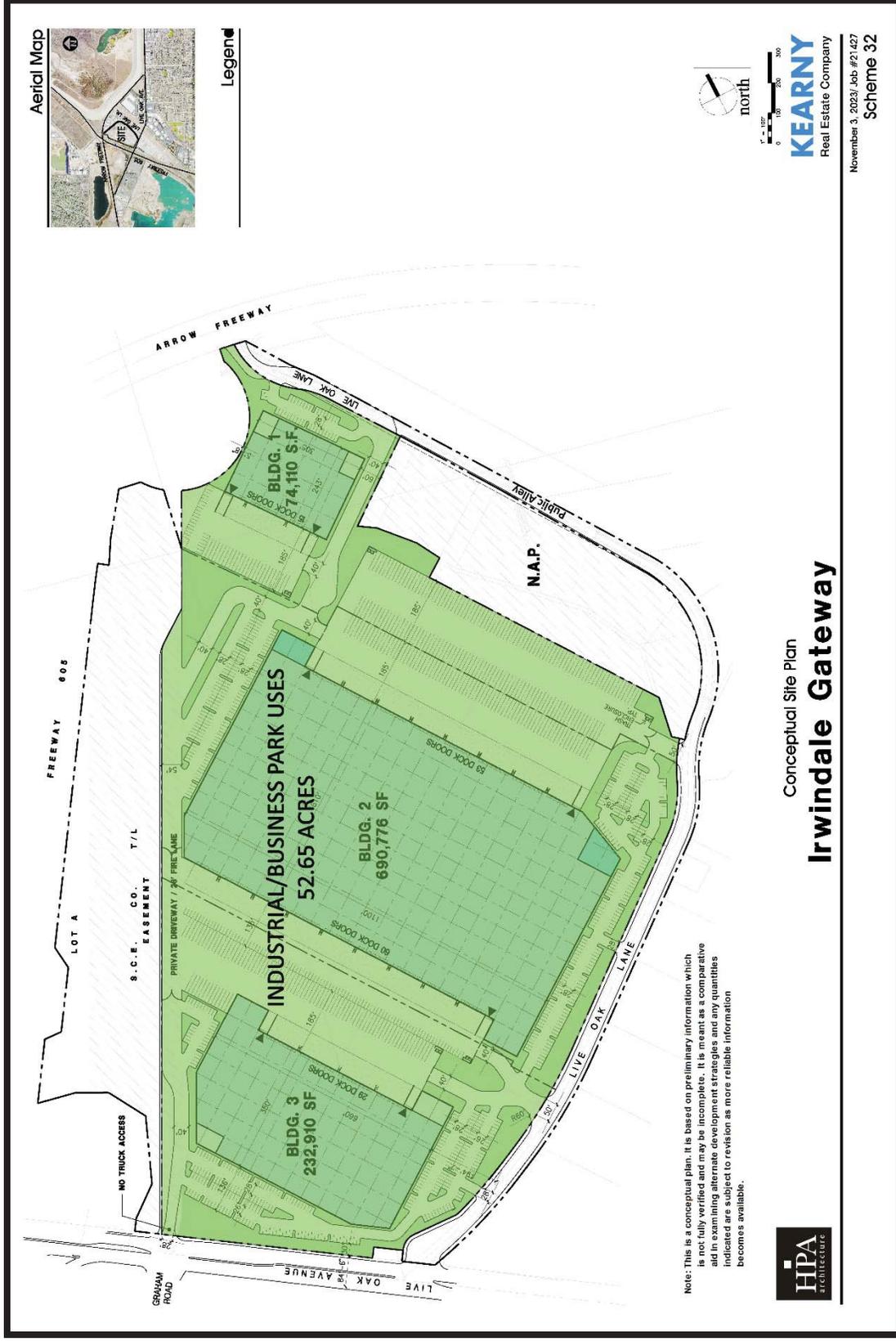


FIGURE 4-1 – LAND USE PLAN NO. 1

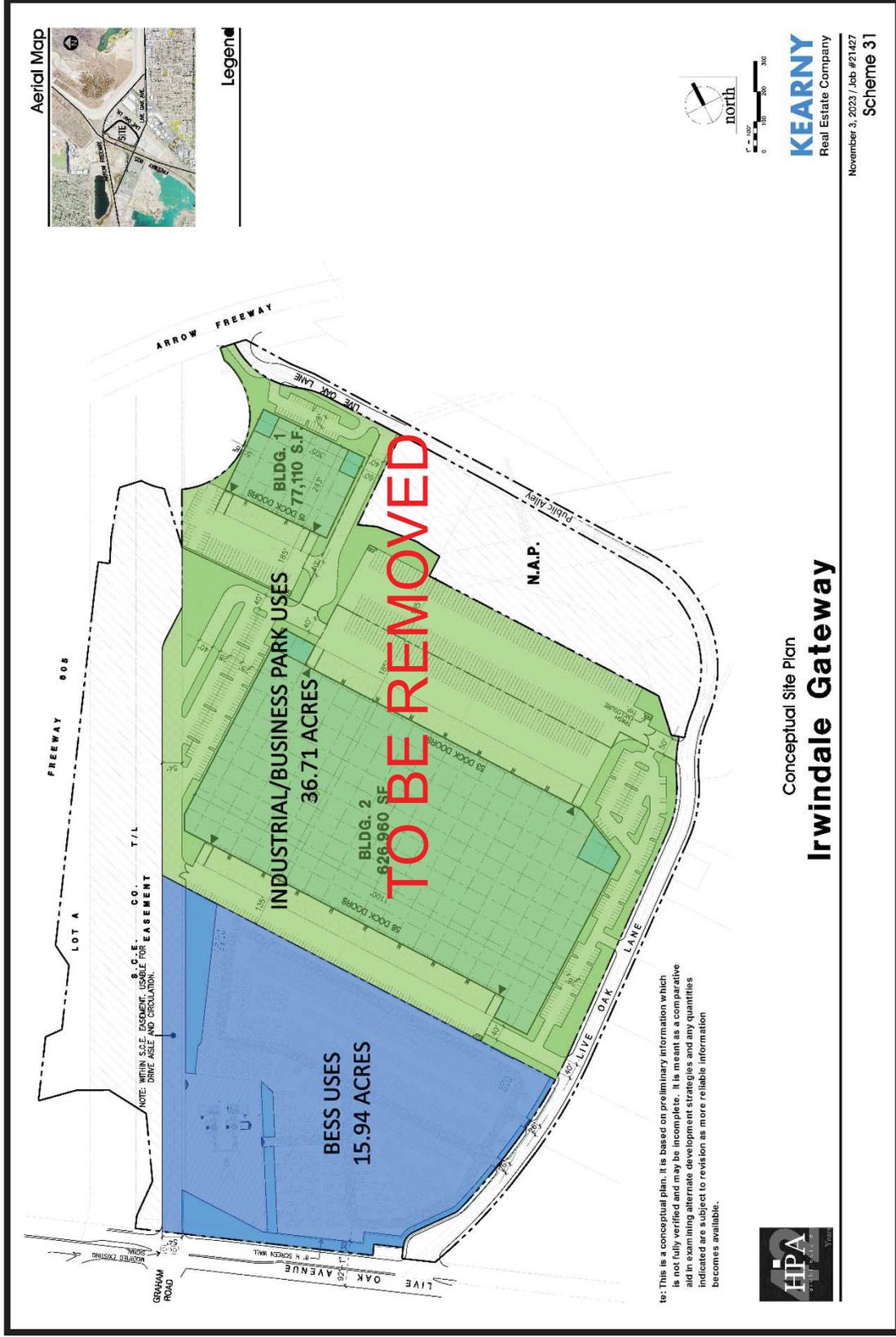


FIGURE 4-2 -- LAND-USE PLAN NO.-2

Transportation and Circulation

V.

5. Transportation and Circulation

5.1 Purpose and Intent

The Circulation Plan for the Specific Plan provides a roadway network to meet the vehicular and non-vehicular needs of employees and visitors and for the transportation of goods to and from the Development Area.

5.2 Vehicular Circulation

The Circulation Plan, as illustrated by Figures 5-1 and 5-2, describes the proposed roadway improvements to accommodate traffic generated by the anticipated Land Use Plan. Vehicular access to the Development Area is provided via Live Oak Avenue, adjacent to the southern boundary of the Specific Plan, and Arrow Highway, which abuts a portion of the northern boundary of the Specific Plan, and Live Oak Lane, a private street extending from Live Oak Avenue to Arrow Highway. Figures 5-1 and 5-2 demonstrate access and on-site circulation for conceptual improvements within the Development Area, ~~with and without any BESS facilities~~. Figures 5-3 and 5-4 illustrate the typical design of circulation improvements planned as part of the development project.

The following facilities and improvements are proposed for the primary components of the Specific Plan's vehicular circulation network:

Live Oak Avenue

Live Oak Avenue forms the southern boundary of the Specific Plan. This public roadway provides east/west access to the Development Area and a connection point to and from I-605. In its fully improved condition, the segment of Live Oak Avenue abutting the Specific Plan features a 106-foot-wide right-of-way ("ROW"), including 34 feet of pavement in the east travel lane (two lanes), 34 feet of pavement in the west travel lane (two lanes), a 16-foot-wide raised center median that reduces to 5-feet to accommodate an 11-foot left turn lane when needed, and a 9-foot-wide sidewalk with 3-feet of landscape terrace on the south side of the street. As part of the Specific Plan Area's development, a 5-foot-wide meandering sidewalk and a 5-foot meandering landscaped parkway is provided along the north side of Live Oak Avenue. The existing travel lanes will remain. Live Oak Avenue is a designated truck route. On-street parking is generally prohibited on both sides of the roadway. All frontage improvements to Live Oak Avenue are required to comply with applicable City of Irwindale requirements, including sight distance requirements, except as may otherwise be provided herein. Live Oak Lane (described below) connects to Live Oak Avenue. One direct driveway will connect to Live Oak Avenue at the signalized intersection with Graham Road, which will also allow temporary truck access during construction. The Live Oak Avenue and Live Oak Lane intersection shall become signalized to alleviate the additional traffic delay generated by the new developments.

Arrow Highway

Arrow Highway abuts the portion of the northern boundary of the Specific Plan where Live Oak Lane intersects with Arrow Highway. This public roadway provides east/west access to the Development Area and to/from I-605. The segment of Arrow Highway abutting the Development Area is improved

with a 100-foot-wide ROW, including 36 feet of pavement in each direction (2 lanes), a 12-foot-wide raised center median, an existing 5-foot-wide sidewalk 3-foot wide landscaped parkway on both sides of the ROW. The Development Area will not have direct vehicular access to Arrow Highway. Instead, two driveways will provide access to Live Oak Lane on the north side of the Development Area, which provides access to Arrow Highway at an unsignalized right-in-right out intersection. Arrow Highway is a designated truck route. Any frontage improvements to Arrow Highway are required to comply with applicable City of Irwindale requirements, including sight distance requirements, except as may otherwise be provided herein.

Live Oak Lane

Live Oak Lane is an existing collector road that connects the development area to either Live Oak Avenue or Arrow Highway. It is a two-lane undivided road generally oriented in a north-south direction. Live Oak Lane is accessible from public streets and provides the primary ingress and egress for all development within the Development Area. Live Oak Lane will have a 60-foot-wide ROW with 40 feet of pavement and 10 feet of sidewalk with parkway on either side of the street. The pavement area includes a 12' travel lane and an 8' wide parking lane on each side of the street. The Live Oak Avenue and Live Oak Lane intersection shall become signalized to alleviate the additional traffic delay generated by the new developments.

Public Alley

A new public alley is provided to connect the northern and southern sections of Live Oak Lane. The public alley is oriented in the north-south direction. The public alley is an undivided two-lane ROW that has varied width, which ranges from 38-feet to 47-feet. One 13-foot-wide travel lane is provided on each direction and an 8-foot parking lane is provided adjacent to the southbound travel lane. On the east side of the public alley is a grass terrace that ranges from 4-feet to 13-feet wide.

Private Driveways and Drive Aisles

Interior private driveways and drive aisles are proposed to connect individual building sites within the Development Area and provide vehicular access to Live Oak Avenue and Live Oak Lane. Private driveways and drive aisles provide vehicular access for automobiles and trucks to parking lots, truck courts, loading dock areas, etc. The locations, alignments, and widths of private driveways and drive aisles will be determined at the time buildings are designed and positioned as part of implementing development projects and are subject to approval of the Director of Engineering.

Interstate 605

I-605 is located immediately west of the Specific Plan and is under the authority of the California Department of Transportation ("Caltrans"). Because the freeway falls completely outside of the Specific Plan Area's boundary, development of the Development Area does not affect Caltrans' plans for the operation and maintenance of I-605. Drivers of I-605 have convenient access to the Development Area via the on-/off-ramps at Live Oak Avenue and on-/off-ramp at Arrow Highway.

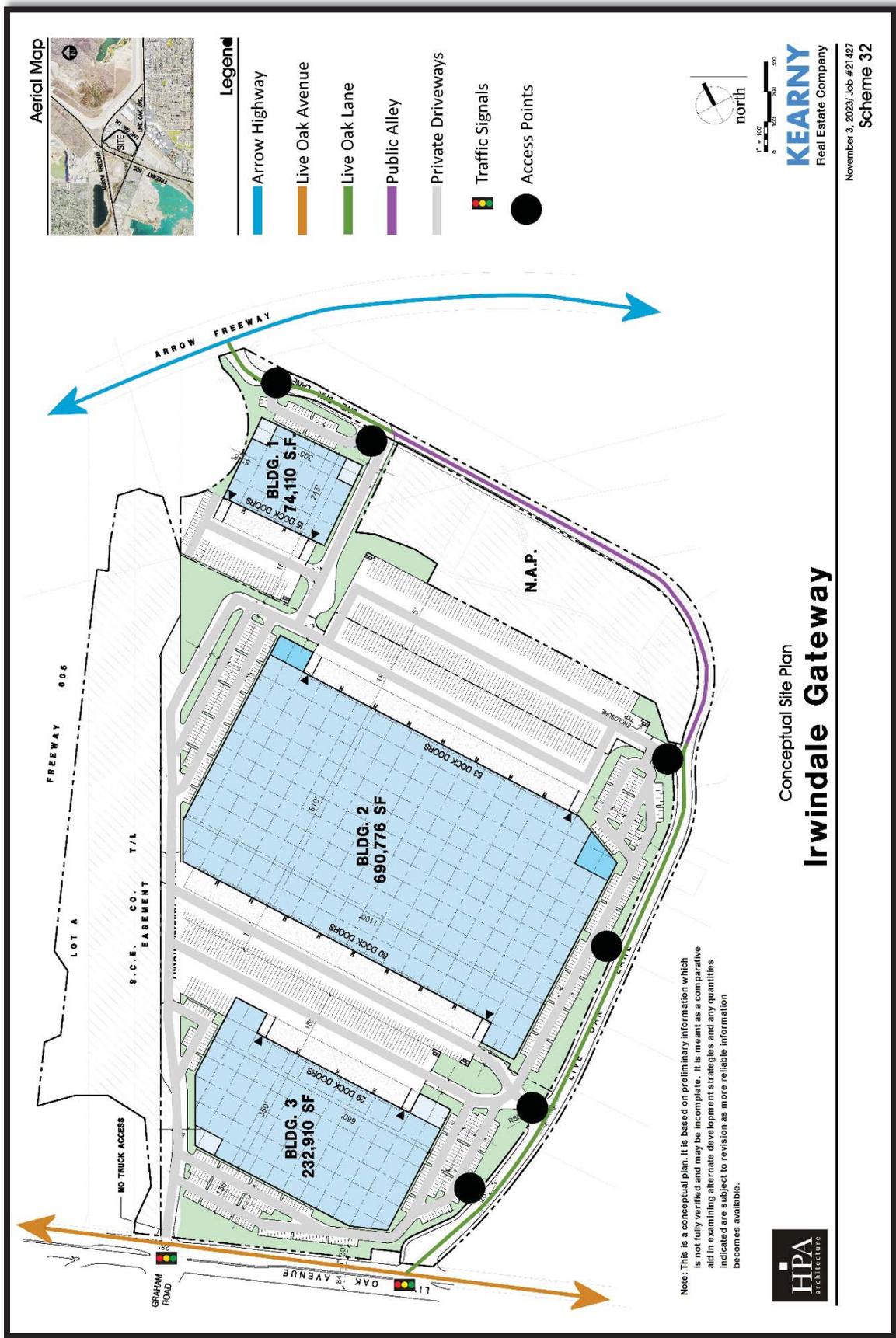


FIGURE 5-2 – CONCEPTUAL CIRCULATION PLAN ~~WITHOUT BESS~~

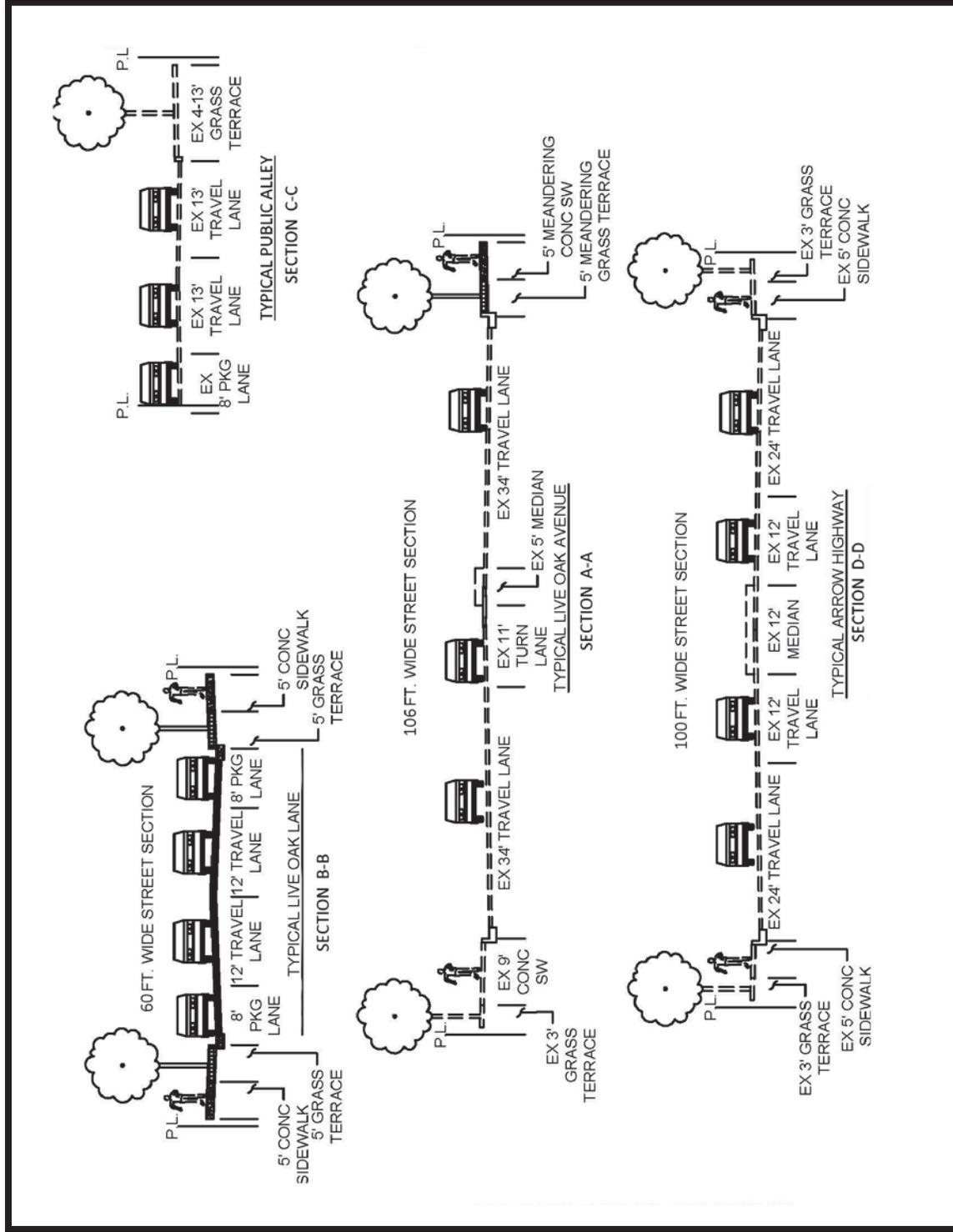
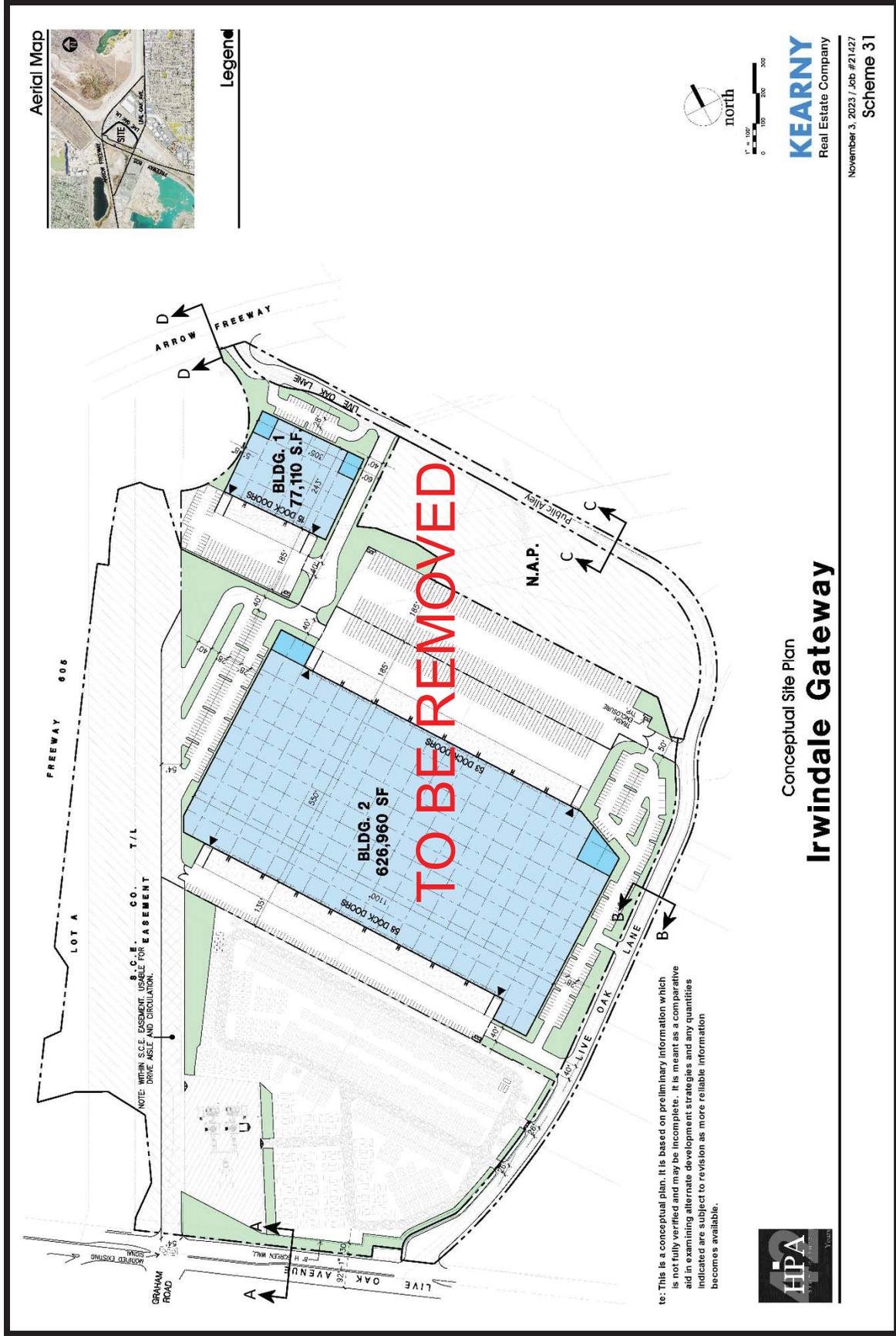


FIGURE 5-3 – TYPICAL ROW CROSS-SECTION



to: This is a conceptual plan. It is based on preliminary information which is not fully verified and may be incomplete. It is meant as a comparative aid in examining alternate development strategies and any quantities indicated are subject to revision as more reliable information becomes available.

FIGURE 5-4 – CROSS SECTION KEY PLAN

5.3 Non-Vehicular Circulation

The Specific Plan provides for convenient pedestrian movement and circulation within the portion of the Development Area that is accessible to the public via an integrated sidewalk network that is to be designed on individual building sites. As illustrated on ~~Figure 5-5, "Conceptual Non-Vehicular Access Plan with BESS" and~~ 5-6 "Conceptual Non-Vehicular Access ~~without BESS,"~~, sidewalks are provided in the public right-of-way along the north side of Live Oak Avenue adjacent to the Development Area. Minimum 5-foot-wide sidewalks are or may be located along both sides of Live Oak Lane to facilitate pedestrian circulation between Arrow Highway and Live Oak Avenue. Crosswalks are designed at signalized intersections to ensure pedestrian safety. Foothill Transit is a public transit agency that serves the City of Irwindale. A bus stop at Live Oak Avenue and Stewart Avenue is located less than a mile from the Development Area and a bus stop at Rivergrade Road and Arrowhead Hwy is located approximately within a mile and a half of the Development Area.

Both Arrow Highway and Live Oak Avenue are Bicycle Priority Corridors in the City's Active Transportation Plan. Development in the Development Area would not impact proposed new bicycle facilities along the roadways.

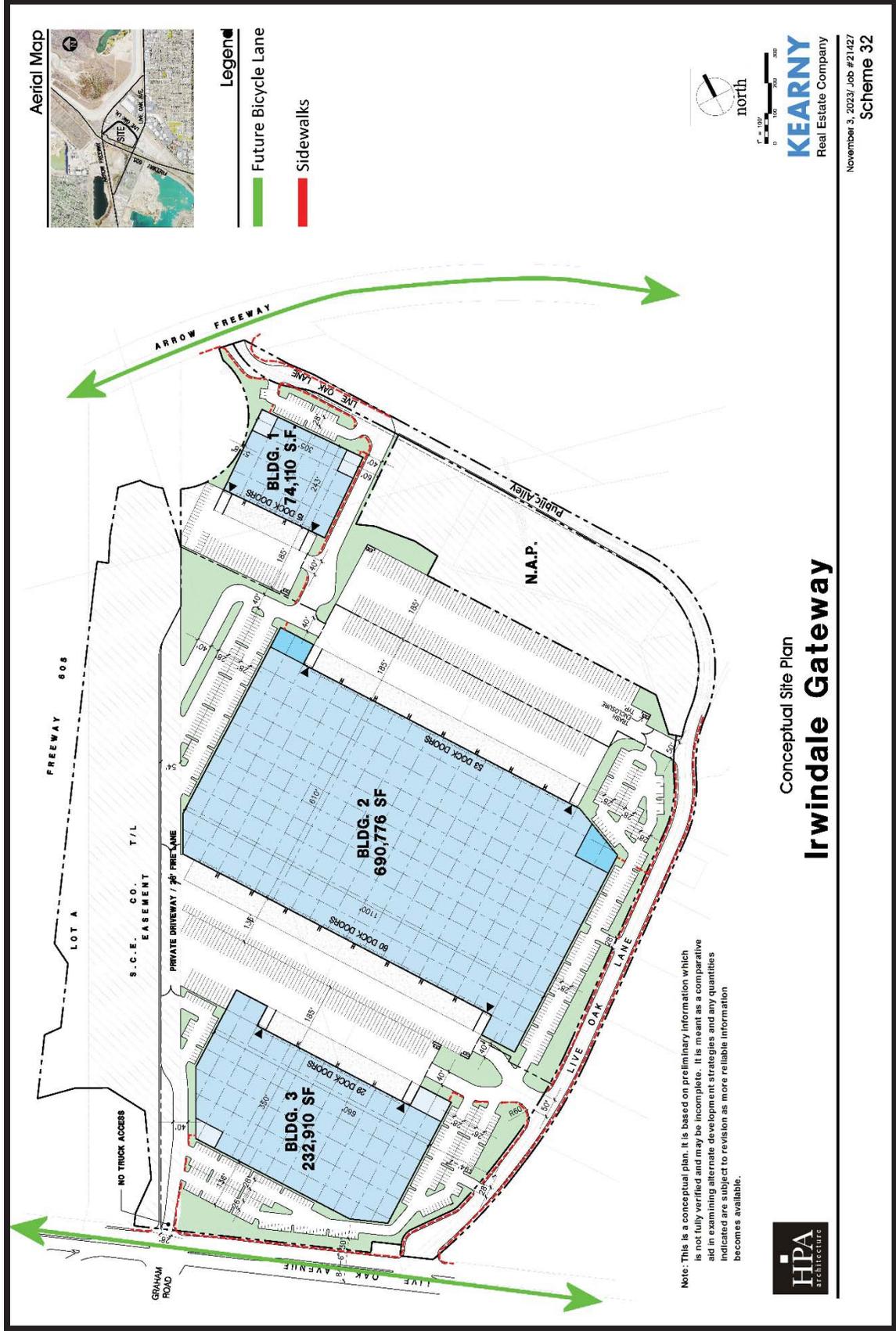


FIGURE 5-6 – CONCEPTUAL NONVEHICULAR ACCESS PLAN **WITHOUT BESS**

Development Standards

VI.

6. Development Standards

6.1 Purpose and Intent

This section establishes development standards applicable to all developments within the Development Area. The standards provided herein work in concert with the architecture and landscape guidelines set forth in Section 7, Design Guidelines, to achieve the vision of the Specific Plan. All new developments and alterations to existing land uses and structures in the Development Area shall be designed, constructed, and established in compliance with the requirements of this Section.

6.2 Definitions

As used herein, the following terms shall have the following meanings:

~~“Battery Energy Storage System” or “BESS”: A battery energy storage system (BESS), is an engineered system of electrical devices and equipment that enable electric energy, such as energy generated from renewable energy sources, like solar and wind, to be stored and then released when the power is needed. BESS components may include: battery modules, battery module enclosures, and Battery Management System (BMSs), bi-directional power inverters and transformers, electrical conductors, overhead and/or underground high voltage lines and supporting pole structures, temperature control and ventilation systems, fire detection and suppression systems, gas detection systems, electrical switching equipment, auxiliary power systems, computer and telecommunications equipment, security systems including but not limited to cameras, lighting, signage, stormwater drainage facilities, perimeter wall or fence, and structures or other supporting ancillary facilities required to facilitate the safe and efficient operation of such systems and equipment or satisfy applicable development standards. Battery modules would be stored in numerous freestanding outdoor battery enclosures or containers that shall not be considered Buildings hereunder or otherwise.~~

~~“Battery Module”: A set of battery cells that are connected together or encapsulated and which are meant to be used either independently or in combination with other modules.~~

~~“Battery Module Enclosure”: A freestanding enclosed container or other structure that contains one or more battery modules. The battery module enclosure is intended to protect the battery modules from external impacts, weather, and the like.~~

~~“Building”: Any structure having a roof supported by columns or by walls and intended for the shelter, housing or enclosure of persons, animals or property of any kind. For purposes of this Specific Plan, a Battery Module Enclosure shall not be considered a Building.~~

~~“Height”: The vertical distance as measured from the highest finished grade adjacent to a Building or structure to the highest point of the same Building or structure, including any building parapet or equipment screening walls.~~

6.3 Permitted Uses

Uses are permitted in the Specific Plan as indicated in Table 6-1. Uses not identified in this table are not permitted unless it is determined by the Community Development Director that a use is similar to other permitted uses, pursuant to the provisions of Section 9.3.

The symbols shown in Table 6-1 are defined as follows:

- **“P”** means the use is permitted by right, subject to applicable development standards and requirements of this Specific Plan.
- **“C”** means the use is conditionally permitted, subject to the approval of a Conditional Use Permit application in accordance with the requirements of the Zoning Code.
- **“A”** means the use is permitted by right, provided the use is ancillary to the primary or conditionally permitted use of the property.
- **“DA”** means the use is subject to a Development Agreement.
- **“X”** means the use is prohibited.

Table 6-1 Allowable Uses

USE	LAND USE PLAN 1	LAND USE PLAN 2	ADDITIONAL NOTES
Professional Offices and Services			
Administrative and professional offices	P	P	
Banks and Financial Institutions	P	P	With and without drive-thru
Data processing and storage centers	P	P	
Industrial, Warehousing, Fabricating, and Manufacturing Uses			
Automobile and truck repair garages	C	C	.
Bakeries and confectionaries, including manufacturing and wholesale	P	P	
Bottling plants	P	P	
Billboards	P	P	Subject to Chapter 17.20 (Billboards) of the IMC
Computer and electronic parts manufacturing and assembly	P	P	
Concrete and concrete products manufacture	C	C	

USE	LAND USE PLAN 1	LAND USE PLAN 2	ADDITIONAL NOTES
Distribution warehouses, general warehouses, and fulfillment centers, for dry goods	P	P	
Distribution warehouses, general warehouses, and fulfillment centers for chilled, cooled, or frozen goods	P	P	A maximum of 387,500 square feet (SF) of chilled, cooled and freezer warehouse space is permitted in the Development Area.
Fabricating and machining of metal, glass, wood, stone, or paper.	P	P	Includes direct-to-consumer sales of goods and/or products on the premises, that are either manufactured, warehoused, or wholesaled on-site.
Food commissaries	C	C	
Manufacturing or processing of food	P	P	Limited to manufacture, wholesale and/or distribution only. Does not include animal slaughter.
Industrial retail sales	A	A	Limited to 15% of the Building's gross sf
Industrial robotics manufacturing and assembly	P	P	
Job printers	P	P	
Joining and assembly manufacturing	P	P	
Machine Shops	P	P	
Machinery manufacture	P	P	
Manufacturing and assembly of products made with wood, stone, fiber, textile, paper, plastic, canvas, casein, cork, clay, metal, pharmaceutical, cosmetic, electrical, glass, and wax	P	P	
Motor vehicle storage	P	P	Indoor storage only
Motor vehicle washing, cleaning, and detailing, includes mechanical or hand motor vehicle washing.	A	A	This ancillary use is not open to the public.

USE	LAND USE PLAN 1	LAND USE PLAN 2	ADDITIONAL NOTES
Plastics, fabrication from and molding	P	P	Includes the light manufacture of products thereof provided all grinding and operations are fully conducted within an enclosed Building
Plating	C	C	
Research & development services/laboratories	P	P	
Scientific instrument and equipment manufacturing or precision materials	P	P	
Self-storage/public	C	C	
Shipping/parcel delivery hub or sorting center	P	P	
Other			
Battery Energy Storage Systems (BESS)	X	DA	Shall be subject to the Irwindale Municipal Code chapter regarding BESS for standards not specified in the Specific Plan. Development shall be limited to the area identified as BESS Area on Figure 4-2.
Telecommunications facilities	C	C	Subject to Chapter 17.12 (Specific Use Regulations) of the IMC

6.4 Development Standards - Industrial and Business Park

Industrial and Business Park Sites shall comply with the development standards listed in Table 6-2.

Table 6-2-Development Standards

Development Standards	Requirement
Site Requirements	
Minimum Lot Size	40,000 square feet
Maximum Floor Area Ratio ¹	0.65
Minimum Setback Requirements²	
Arrow Highway Setback	
Building	20 ft.

Development Standards	Requirement
Drive Aisle and Parking ³	12 ft.; 10 ft. adjacent to dedicated turn pockets
Live Oak Avenue Setback	
Building	20 ft.
Drive Aisle and Parking ³	12 ft.; 10 ft. adjacent to dedicated turn pockets
Live Oak Lane Setback	
Building	10 ft.
Drive Aisle and Parking ³	10 ft.
Allowable architectural encroachments into setbacks (cornices, eaves, canopies, etc.) ⁴	5 ft.
Building Requirements	
Maximum Building Height	60 ft.
Minimum Distance Between Buildings	20 ft.
Minimum Drive Aisle/Parking Space Separation	
Adjacent to building wall	10 ft.
Adjacent to screen wall/fence	5 ft.
Within screened/enclosed yards	0 ft.
Minimum Parking and Access Requirements	
Automobile Parking Ratio	Per Chapter 17.16 (Parking and Loading Standards) of the IMC
Automobile Parking Stall Size	9 ft. x 19 ft. (standard) 8 ft. x 15 ft. (compact) ⁵
Truck Parking/Storage Stall Size	10 ft. x 55 ft.
Drive Aisle Width	26 ft.
Other Development Standards	
Walls and Fences	4 ft. minimum height 14 ft. maximum height
Minimum Landscaped Area	Per Section 6.6
Site Lighting	Light fixtures shall not exceed a height of 35'. Exterior lighting shall produce a maximum initial illuminance of no greater than 0.5 horizontal and vertical footcandles at the site boundary and beyond
Signs	In accordance with Chapter 17.19 (Signs) of the IMC

Notes

¹ The FAR maximum shall be calculated as a ratio of (1) the aggregate square footage of the Building(s) located on any portion of the Development Area developed for Industrial and Business Park Uses divided by (2) the total square footage of such development site minus areas of public right of way. Under Land Use Plan 1 this would result in 1,490,732 s.f. (52.65 acres x 43,560 sf/acre x 0.65) and 1,039,407 s.f. (36.71 acres x 43,560 sf/acre x 0.65) under Land Use Plan 2.

² Minimum setback area shall be that distance measured between the face of the building wall or closest side of structure to the edge of the public right of way.

³ Entire setback shall be landscaped. Landscaping shall include trees, shrubs, groundcovers, and/or vines and may include walkways, benches, trellises, thematic fencing/walls, and related amenities.

⁴ Only permitted where adequate emergency access can be maintained.

⁵ Compact spaces shall comprise no more than 25% of all required automobile parking spaces.

6.4.1 Other Development Standards (Industrial and Business Park)

In addition to the development standards required by Table 6-1, the following standards apply to Industrial and Business Park Sites:

1. Manufacturing and processing activities shall be conducted within a wholly enclosed building.
2. Outdoor storage of materials and equipment shall be ancillary to a permitted use. Outdoor storage shall be fully screened and trucks and vehicles shall be stored within enclosed areas.
3. All loading/unloading of trucks shall occur within the truck courts. Trucks and service vehicles shall have clear and convenient access into and within truck courts or loading areas of every building and shall not be located to disrupt vehicular and pedestrian circulation. The loading/unloading of trucks within the right-of-way shall be prohibited.
4. Loading docks and truck yards shall be visually screened from public streets by walls, fencing, landscaping, and/or other screening features or barriers (such as berms) with a height of 14 feet, as measured from the interior side of the barrier.
5. Ground-mounted exterior mechanical and electrical equipment, heating, ventilating, air conditioning, tanks, and other mechanical and electrical devices shall be screened and treated with a neutral color or obscured by landscaping when visible from Arrow Highway, Live Oak Avenue, or Live Oak Lane.
6. Roof-mounted equipment shall be screened when visible from Arrow Highway, Live Oak Avenue, or Live Oak Lane. Screening may be accomplished with the building parapet or the use of a roof mounted equipment screen. Screening materials shall be compatible with the design and architecture of the building.
7. Entry gates shall be positioned to allow enough distance for the stacking of at least one (1) 5-axle truck in front of each gate without any portion of the truck extending into the public right-of-way.
8. Locate and design service entrances so they do not interfere with owner/tenant/customer access.
9. Trash and refuse collection areas shall comply with the provisions of Chapter 17.13 (Site Planning and General Development Standards) of the IMC.
10. Prior to issuance of a building permit, a Comprehensive Sign Program shall be approved by the City pursuant to Chapter 17.19 (Signs) of the IMC.

~~6.5 Development Standards – Battery Energy Storage Systems~~

~~Battery Energy Storage System developments shall comply with the development standards contained in Table 6-3.~~

Table 6-3 Development Standards

Development Standards	Requirement
Site Requirements	
-Maximum Lot Coverage ¹	65%
Minimum Setback Requirements²	
Live Oak Avenue Setback	
-BESS equipment or other enclosed structure	20 ft.
-Drive Aisle and Parking ³	12 ft.; 10 ft. adjacent to dedicated turn pockets
Live Oak Lane Setback	
-BESS equipment or other enclosed structure	10 ft.
-Drive Aisle and Parking ³	10 ft.
BESS and Building Requirements	
Maximum BESS Equipment Height	12 ft.
Maximum Building Height	15 ft.
Minimum Parking and Access Requirements	
Automobile Parking Ratio	
-BESS areas	Four (4) parking spaces for the entire BESS facility
Automobile Parking Stall Size	
	9 ft. x 19 ft. (standard)
	8 ft. x 15 ft. (compact) ⁴
Minimum Drive Aisle Width	20 ft.
Other Development Standards	
Walls and Fences	14 ft. maximum height
Minimum Landscaped Area	Per Section 6.6
Site Lighting	Light fixtures shall not exceed a height of 35 ft.
Signs	In accordance with Chapter 17.19 (Signs) of the IMC
Maximum Height of BESS Substation Dead-end Structure ⁵	65 ft.
Maximum Height of Interconnection Poles to Southern California Edison Substation (along Live Oak Ave).	The electrical tie-line shall be undergrounded unless the applicable agency(ies), including but not limited to Southern California Edison or California Independent System Operator (CALISO), reject the proposed underground line. If the applicable agency(ies) require an overhead tie-line, the Applicant shall be subject to a Zone Variance application per IMC Chapter 17.32.

Notes

¹Lot coverage shall be calculated as a ratio of (1) the total aggregate square footage of area within any development site of the footprint of Battery Module Enclosures, inverters, transformers, and any ancillary buildings divided by (2) the total square footage of such development site.

- ~~² Minimum setback area shall be that distance measured between the face of the building wall or closest side of structure to the edge of the public right of way.~~
- ~~³ Entire required setback shall be landscaped. Landscaping shall include trees, shrubs, groundcovers, and/or vines and may include walkways, benches, trellises, thematic fencing/walls, and related amenities.~~
- ~~⁴ Compact spaces shall comprise no more than 25% of all required automobile parking spaces.~~
- ~~⁵ The dead-end structure is the onsite substation termination of the transmission line and highest structure for the onsite substation.~~

~~6.5.1 Other Development Standards (BESS)~~

~~In addition to the development standards required by Table 6-2, the following standards apply to Battery Energy Storage System developments:~~

- ~~1. A BESS facility shall be screened with an 8-foot high wall where in sight of proximal portions of Live Oak Avenue or Live Oak Lane at Specific Plan build-out.~~
- ~~2. The ground surface of a BESS facility shall be covered with gravel, asphalt, concrete, or other compatible materials.~~
- ~~3. BESS collector substation metal structures and high voltage line support poles shall be galvanized steel or finished with another low reflective neutral colored surfacing when visible from Arrow Highway, Live Oak Avenue, or Live Oak Lane.~~
- ~~4. A BESS facility may include an onsite "collector" substation that would connect via a 230 kV underground electric tie-line to a Point of Interconnection (POI) at the existing Southern California Edison ("SCE") Edison Rio Hondo Substation located south of Live Oak Avenue. The electrical tie-line shall be undergrounded unless the applicable agency(ies), including but not limited to Southern California Edison or California Independent System Operator (CALISO), reject the proposed underground line. If the applicable agency(ies) require an overhead tie-line, the Applicant shall be subject to a Zone Variance application per IMC Chapter 17.32. The overhead line would transition to underground at a transition pole. The underground portion of the electric tie-line would consist of conduits containing electric power cables, fiber optic communications cable, and a grounding conductor within an approximately 3'-0" wide and 3'-0" deep, high-strength concrete encasement that would be a minimum of 3'-0" below the surface. Impacts related to the undergrounding of the electric tie-line would not result in a net increase of impacts of the proposed projects.~~
- ~~5. A BESS facility may include, but is not required to include, a single-story warehouse-type building for storage of parts used for maintenance. Any such single-story warehouse-type building shall be consistent with the Design Guidelines set forth in Chapter 7 of this Specific Plan.~~
- ~~6. Prior to issuance of a building permit, a Comprehensive Sign Program shall be approved by the City pursuant to Chapter 17.19 (Signs) of the IMC.~~

6.6 Landscape

The following requirements shall apply to all site improvements within the Development Area, regardless of which Land Use Plan is implemented.

6.6.1 General Requirements

1. Minimum Site Landscaping. A minimum of 10% of the site area shall be landscaped.
2. Parking Lot Landscaping. A minimum of 20% of the total required landscape shall be provided in the parking lot; ~~provided, however, that this requirement shall not apply to a development site within the Development Area that is being used for BESS.~~
3. Required Areas. All setbacks, parkways, open areas, plazas, paseos, and non-work areas that are visible from a public street/alley or from a parking lot available to the general public shall be landscaped.
4. Landscape Coverage Requirement. Shrubs, groundcover, and other plant material shall cover all areas that are visible from a public street/alley or from a parking lot available to the general public and that are not occupied by structures, parking areas, storage, trash enclosures, driveways, and sidewalks at the time of issuance of a Certificate of Occupancy. Embellished pavement, fountains, and similar hardscape materials may, in part, be substituted for the required landscaping through the Site Plan and Design Review process.
5. Parkway-Adjacent Planting and Maintenance. Parkways located between the sidewalk and the edge of development shall meet the following requirements:
 - a. The ground surface shall contain shrubs, mulch, or ground cover to provide coverage within two years.
 - b. If a wall or fence separates the development from the street, planting vines or espalier shrubs shall be incorporated into the planting design.
6. Required Landscaping for Loading Areas. Loading areas shall incorporate landscaping to provide screening if visible from the public right-of-way, adjacent uses, and pedestrians.

6.6.2 Trees

1. Perimeter landscaping shall include one street tree for each 30 lineal feet of street frontage.
2. Within parking areas, one tree for each 25 parking spaces shall be provided.
3. All trees planted along a street frontage shall be of a minimum 15-gallon size. Palm trees shall have a minimum brown trunk height of six feet.
4. All other trees planted in required landscaped areas shall be a minimum of five-gallon size or have a brown trunk height of three feet.

Design Guidelines

VII.

7. Design Guidelines

7.1 Purpose and Intent

The Design Guidelines presented in this section establish the quality and character of the built environment for the master-planned development of the Specific Plan. The objectives of the Design Guidelines are:

1. To describe the thematic elements and the construction quality expected for the Development Area.
2. To provide the City of Irwindale with assurance that the Development Area will be developed in accordance with the quality and character described within this Specific Plan.
3. To serve as a guide to developers, builders, engineers, architects, landscape architects, and other professionals involved with implementing development in the Development Area in order to achieve and maintain the desired design quality.
4. To provide an aesthetic benchmark for the City of Irwindale to use in their review of future implementing projects within the Development Area.
5. To steer the Development Area to convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
6. To encourage the implementation of energy efficient design features in Buildings that can be implemented in the site planning, design, and construction phases of the Development Area to minimize waste deposited at landfills, decrease energy use and fossil fuel consumption, and reduce domestic water consumption.

7.2 Applicability

The Design Guidelines presented in this chapter apply to all development within the Specific Plan Area, ~~except Battery Module Enclosures and associated substation and electrical equipment (collectively, "BESS Facilities"), for which uses, only the development regulations shall apply.~~ Where these Design Guidelines are silent, ~~except with respect to BESS Facilities,~~ the City's Commercial and Industrial Design Guidelines shall prevail.

7.3 Site Planning

This section sets forth general site planning guidelines that address unique considerations associated with development within the Development Area.

1. Locate the office portion of Buildings at the corner(s) of the Building. For Buildings adjacent to Live Oak Avenue or Live Oak Lane, orient the office toward the road to provide visual interest from the public roadway.
2. Orient and screen elements such as trash enclosure areas, loading bay doors, and service docks in ways that minimize their visibility from Live Oak Avenue or Live Oak Lane.
3. Use solid walls, screened fences, landscaping, or other visual barriers to visually screen truck courts and loading docks from public view from Live Oak Avenue or Live Oak Lane, where possible.

4. Site design shall specifically address the needs of pick-up, delivery, and service vehicles related to Industrial and Business Park Uses.
 - a. Design loading areas to provide for tractor trailer backing and maneuvering on-site and not from a public street.
 - b. Provide appropriate on-site service vehicle parking/turnouts in an efficient, non-obtrusive location appropriate to the scale and needs of the development.
 - c. Loading vehicles, when parked, shall not impede normal traffic flow.

Conceptual site plans are shown in Figures 7-1 and 7-2.

7.4 Building Architecture

7.4.1 Architectural Design

Buildings should be characterized by simple and distinct cubic masses with interlocking volumes of wall planes, colors, and materials to create visual appeal, aesthetically pleasing proportions, and strong shadow patterns. Colors, materials, and textures will be mixed to create interest.

A Spanish contemporary influence is required throughout the Development Area. All design elements of Buildings shall be compatible (but not identical) in character, massing, and materials in order to promote a clean and contemporary style. Creativity is encouraged in building design, with care taken to maintain a sense of similarity among all Buildings to reinforce a unified image within the Development Area. Generally, Buildings within the Development Area shall not be overly “trendy” or strongly historical; however, contemporary Spanish thematic elements visible from public streets, such as arched entryways and windows, and other subtle references to the history of the region are acceptable. Architectural styles should complement, not detract from, the general architectural character found in the City of Irwindale.

7.4.2 Building Form

Building form is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a Building’s “form.” Building forms are especially important for building elevations that face the following view corridors:

1. Building façades that face and are immediately adjacent to and visible from Live Oak Avenue.
2. Building façades that face and are immediately adjacent to and visible from Arrow Highway.
3. Building façades that face and are immediately adjacent to and visible from Live Oak Lane.

The following guidelines apply to all Buildings within the Development Area to ensure that structural development is visually appealing and inviting to pedestrians and motorists:

1. Use geometric forms to constitute the overall building form. Rectangular forms are encouraged to promote balance, rhythm, and visual interest. Layering of forms creates detail, depth and shadow and is strongly encouraged. However, avoid arbitrary, complicated building forms.
2. Articulate building planes visible from Live Oak Avenue, Arrow Highway, I-605, and/or Live Oak Lane by changes in exterior building materials, color, texture, and decorative accents.

Articulated features (e.g., pop-outs and recesses, breaks, overhangs, height changes, etc.) should be used to create a pedestrian scale at primary Building entries.

3. Modulation and variation of building masses between adjacent Buildings visible from Live Oak Avenue, Arrow Highway, and Live Oak Lane is encouraged. This includes varied parapet heights and change of building plane, in plan.
4. Generous use of windows and doors in the office areas and articulation and different paint colors to break up Building walls and bulk.
5. Design each Building to have a well-defined entry with careful roof and façade articulation to create visual interest and scale, including changes in massing, color, and/or building materials.
6. Recess or cover pedestrian and ground-level Building entries by architectural projections or roofs in order to provide shade and visual relief.
7. Design Buildings to have a base and cornice expression. These expressions can be accomplished through the use of clean, simplistic, and not overly complicated architectural and trim detailing on Building façades, and changes of material/color or recesses.
8. Materials applied to building elevations should turn the corner of the Building to a logical termination point in relation to architectural features or massing.

Conceptual elevations demonstrating acceptable building form are illustrated in Figure 7-3 below.

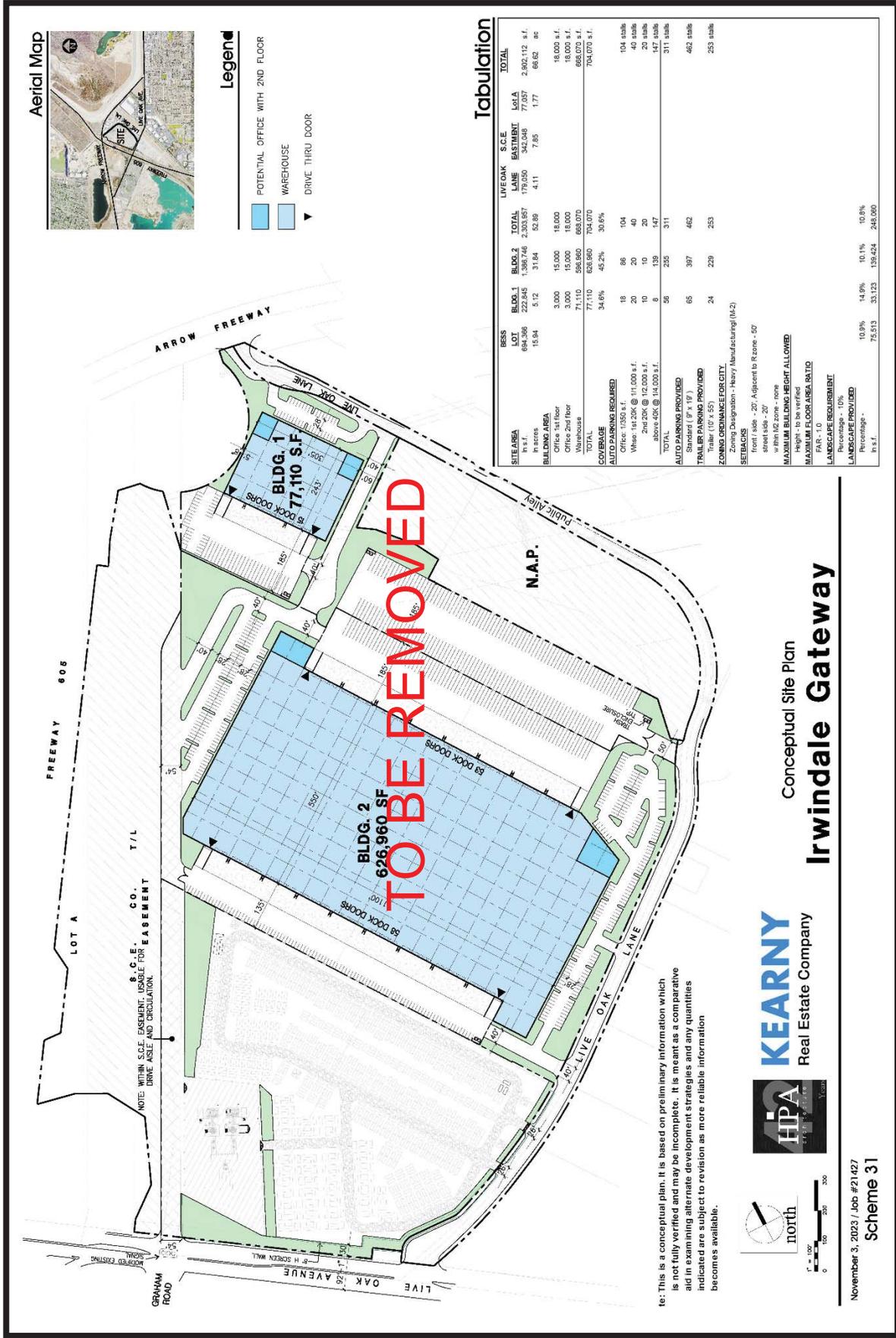


FIGURE 7-1 -- CONCEPTUAL SITE PLAN WITH BESS

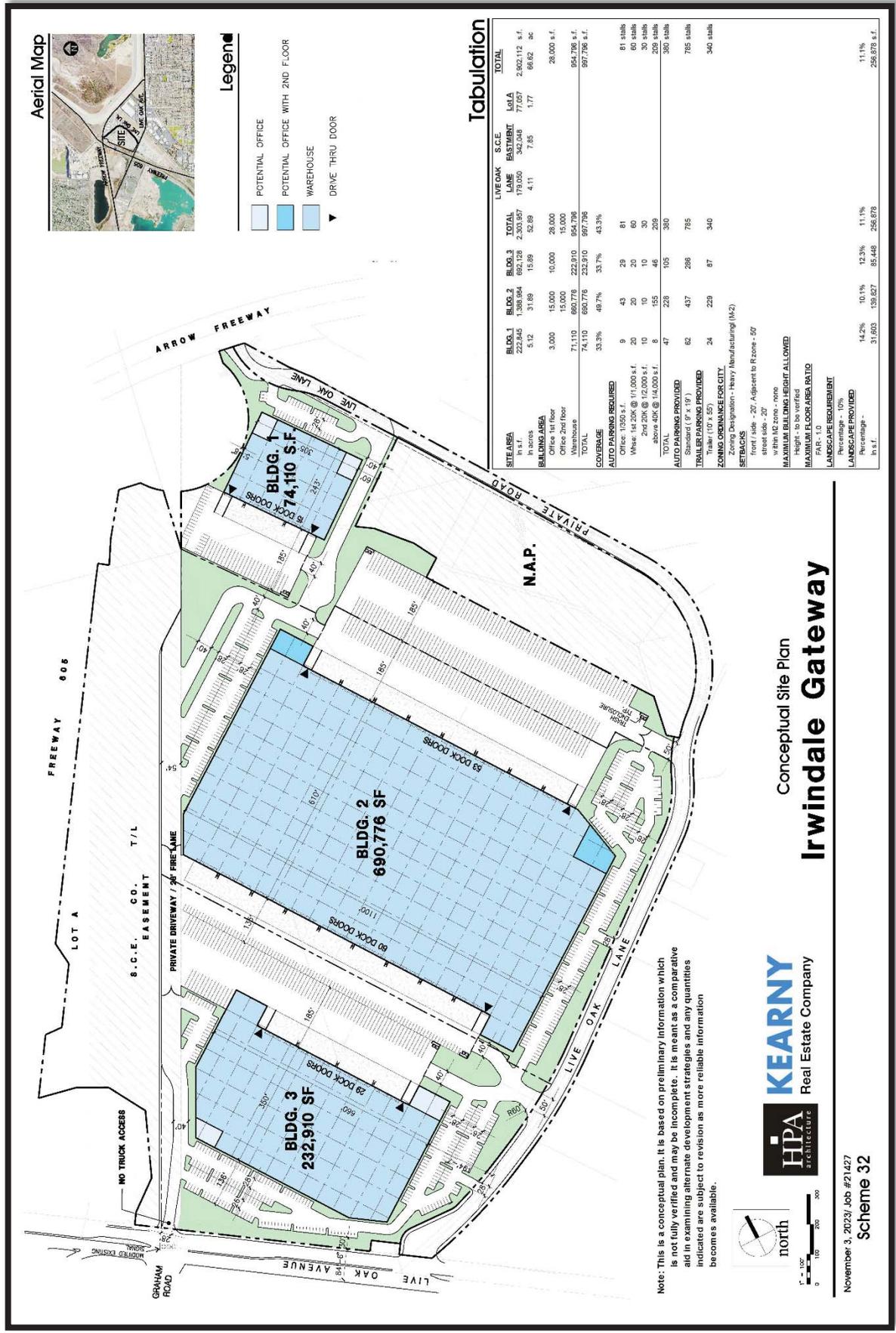


FIGURE 7-2 – CONCEPTUAL SITE PLAN ~~WITHOUT-BESS~~

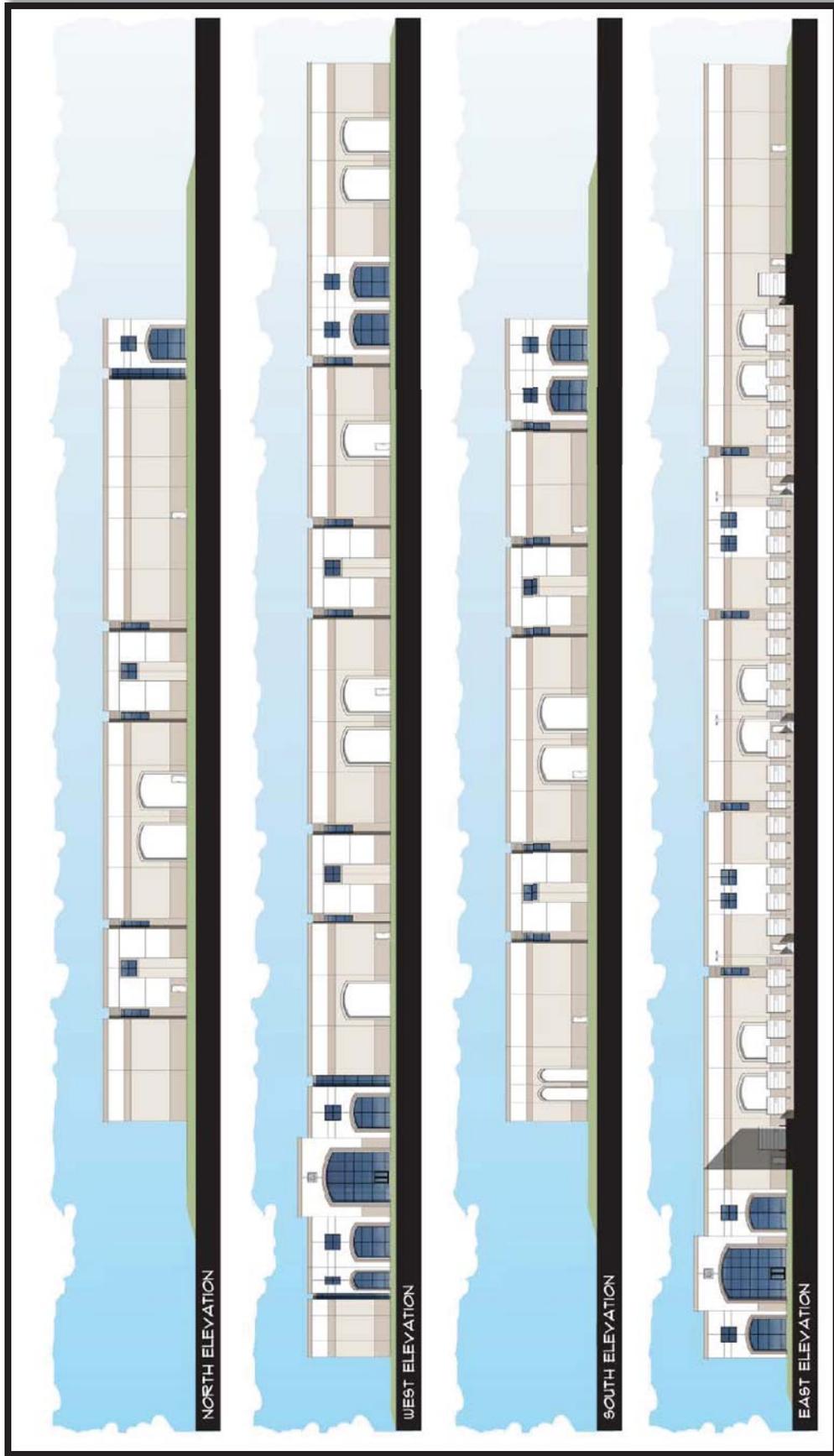


FIGURE 7-3 – CONCEPTUAL BUILDING FORM

7.4.3 Building Materials, Colors, and Textures

The use of complementary materials and colors for Buildings plays a key role in developing a clean, contemporary visual environment. Accordingly, the selected exterior materials, colors, and textures for Buildings should complement one another among all Buildings within the Development Area. Slight variations in materials, colors, and/or textures from Building to Building are encouraged to provide visual interest.

1. Appropriate primary exterior building materials include stucco, concrete, and similar materials, including concrete tilt-up panels. Accent primary materials used on Building facades that are visible from public rights of way through the use of secondary materials such as glass or glazing units, glass block, natural or fabricated stone, brick, metal, and tile or tile panel systems.
2. The use of metal and/or glass fabrications for curtain wall areas are appropriate.
3. Trim details may include metal finished in a consistent color, plaster, contemporary shaped foam, or concrete elements finished consistently with the building treatment. Use of overly extraneous “themed” detailing, like oversized or excessively shaped foam cornice caps, foam molding, and window detailing is discouraged.
4. Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
5. Primary exterior Building colors are encouraged to be light and warm tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around Building entrances and near outdoor gathering spaces.
6. Bright primary colors, garish use of color and arbitrary patterns or stripes are discouraged, except in signage logos.
7. Exposed downspouts, service doors and mechanical screen colors should be the same color as the adjacent Building wall.

The Suggested Material and Color Palette shown in Figure 7-4 below is for conceptual purposes only. Other colors that are consistent or similar in nature to the colors provided in the Suggested Color Palette may also be considered as appropriate.

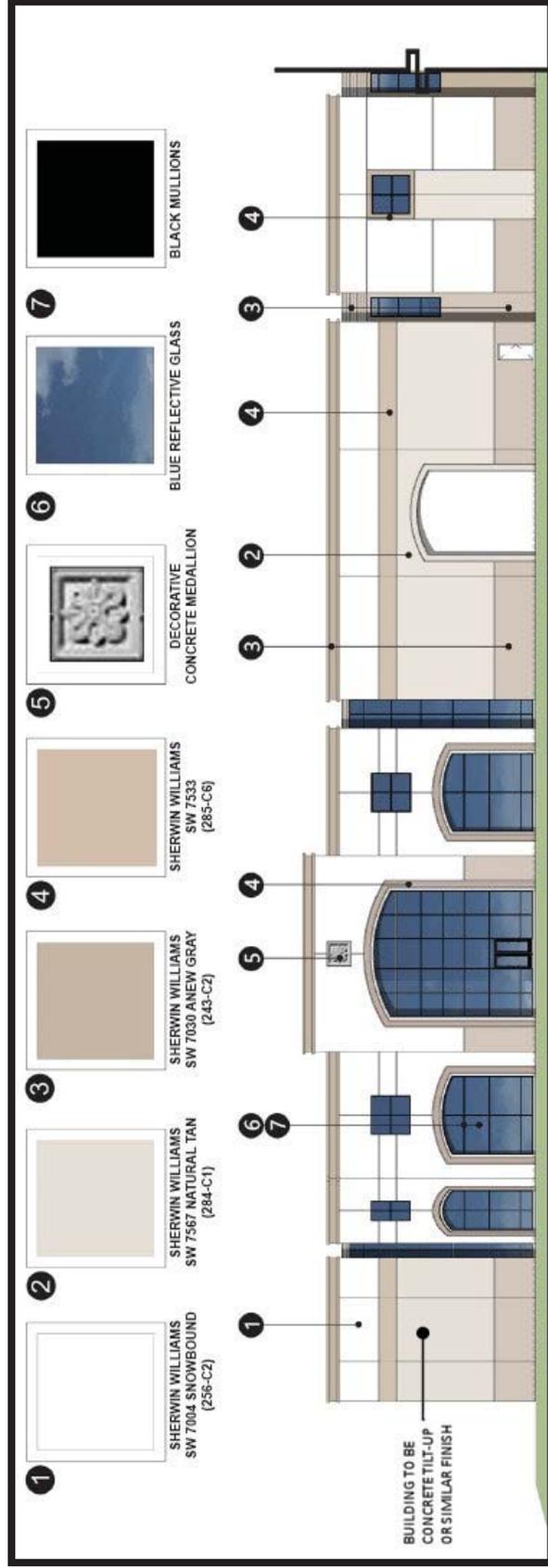


FIGURE 7-4 – COLOR AND MATERIAL PALETTE

7.4.4 Windows and Doors

The patterns of openings – windows and doors (excluding trailer-loading dock doors) – are to correspond with the overall rhythm of the Building and be consistent in form, pattern, and color. Guidelines for windows and doors used in the Development Area are as follows:

1. Introduce recessed window and door openings to enhance the visual play of light and shadow.
2. When possible, place the layout of doors and windows on individual Building façades in a repetitive pattern to create continuity.
3. Use consistent styles, forms and colors of windows.
4. Gold or unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.
5. Use clear or colored glass with medium to high performance glazing. Silver mirrored glass is prohibited.
6. Clearly define all pedestrian entrances to Buildings by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways, and unarticulated entry alcoves are discouraged.

Figure 7-5- Windows and Doors



7.4.5 Functional Elements

Carefully consider the design and location of functional elements common to all Buildings. Examples of functional elements include loading doors, service docks, ground or wall-mounted equipment, rooftop equipment, rain gutters and downspouts, and trash enclosures.

Loading Doors and Service Docks

1. Orient and screen loading doors, service docks, and equipment areas so they are not easily visible from Live Oak Avenue, Arrow Highway, Live Oak Lane, and publicly accessible locations within the Irwindale Gateway Development Area. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the Building or by any effective combination of walls, fences, landscaping, and berms.
2. No direct loading or unloading activity is permitted to take place from Live Oak Avenue, Arrow Highway, or Live Oak Lane. Trucks and service vehicles shall have clear and convenient access

into and within truck courts or loading areas of every building within the Development Area and should not disrupt vehicular and pedestrian circulation.

3. Separate loading docks and truck courts from visitor and customer parking areas and pedestrian circulation areas (e.g., walkways) utilizing walls, fences and/or landscaping.
4. Design truck and service vehicle entries to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected.

Ground or Wall-Mounted Equipment

1. Locate electrical equipment rooms within a Building. Pop-outs or shed-like additions are discouraged, unless fully incorporated in the architectural concept of the Building.
2. Ground-mounted equipment, including but not limited to mechanical equipment, electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, but specifically excluding electrical transformers, shall be screened to limit visibility from off-site public viewing areas. Screening may be accomplished with site walls or landscape elements that are consistent with these Design Guidelines.
3. Wall-mounted items, such as roof ladders or electrical panels, shall not be located on the Building façade facing adjacent public roads when alternative locations are practical and safe. Wall-mounted items should be screened or incorporated into the architectural elements of the Building so as not to be visually obvious from public streets or other publicly accessible areas within or adjacent to the Development Area.

Rooftop Equipment

1. Rooftop equipment, including but limited to mechanical equipment, electrical equipment, storage tanks, cellular telephone facilities, satellite dishes, skylights, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened to limit visibility from public roads or visitor parking lots within the Development Area.
2. Rooftop screens (i.e., parapet walls) shall be integrated into the architecture of the main Building.
3. Wood finished rooftop screens are prohibited.

Trash Enclosures

1. All outdoor refuse containers shall be screened within a permanent, durable enclosure and should be oriented so they are not easily visible from public roads or other public viewing areas.
2. The design of trash enclosures shall reflect the architectural style of adjacent Buildings and use similar, high-quality materials.
3. Refuse collection areas shall be located behind or to the side of Buildings, away from the Building's main entrance.
4. All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted storm water runoff from these structures.

7.5 Landscape Architecture

The Specific Plan incorporates landscaping as a visual amenity along its perimeter to soften and minimize the perceived scale of development. Figures 7-6 and 7-7 illustrate the location of landscape amenities around the Development Area.

These Landscape Design Guidelines complement the existing setting of Irwindale, Southern California climate and local soil conditions, ease of maintenance, and water conservation. Water-efficient and drought-tolerant plant materials shall be placed throughout the Specific Plan and 'smart' computer-controlled irrigation systems shall be used to reduce water use to the minimum level necessary. These Landscape Design Guidelines promote an identity for the Development Area that is visually appealing and sensitive to the environment.

Although specific design information is presented in these Landscape Design Guidelines, these Guidelines are not intended to establish a set of rigid landscaping requirements for the Development Area and it is recognized that, at times, there may be a need to adapt these guidelines to meet certain pad-specific or building user identity requirements. As such, these Landscape Guidelines are intended to be flexible. However, it is critical to the Development Area's long-term design integrity that any deviations from these Landscape Design Guidelines are in keeping with the spirit of the core elements of the overall theme described herein to ensure a cohesive and unified landscape concept across the Development Area.

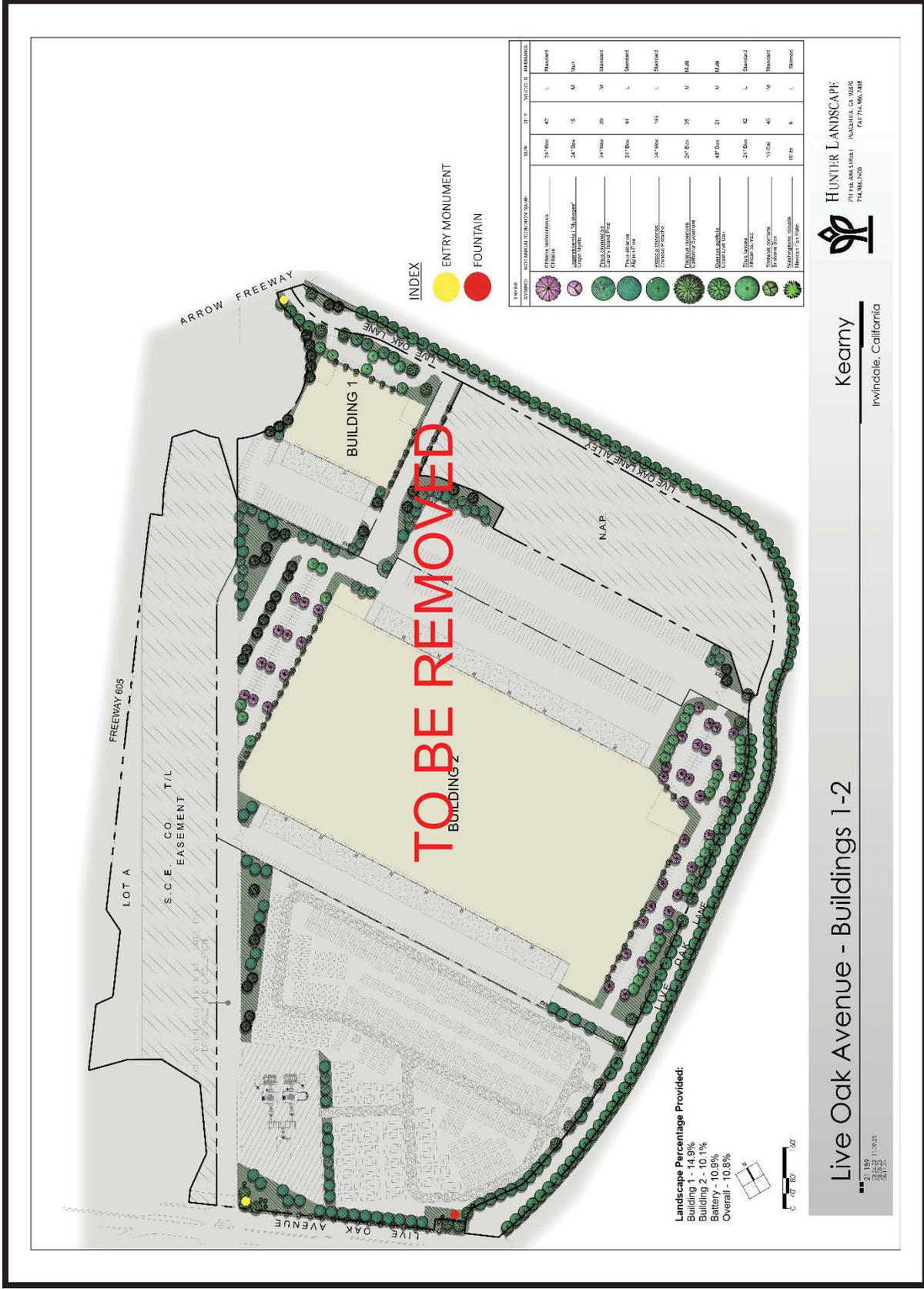


FIGURE 7-6 CONCEPTUAL LANDSCAPE PLAN WITH BESS

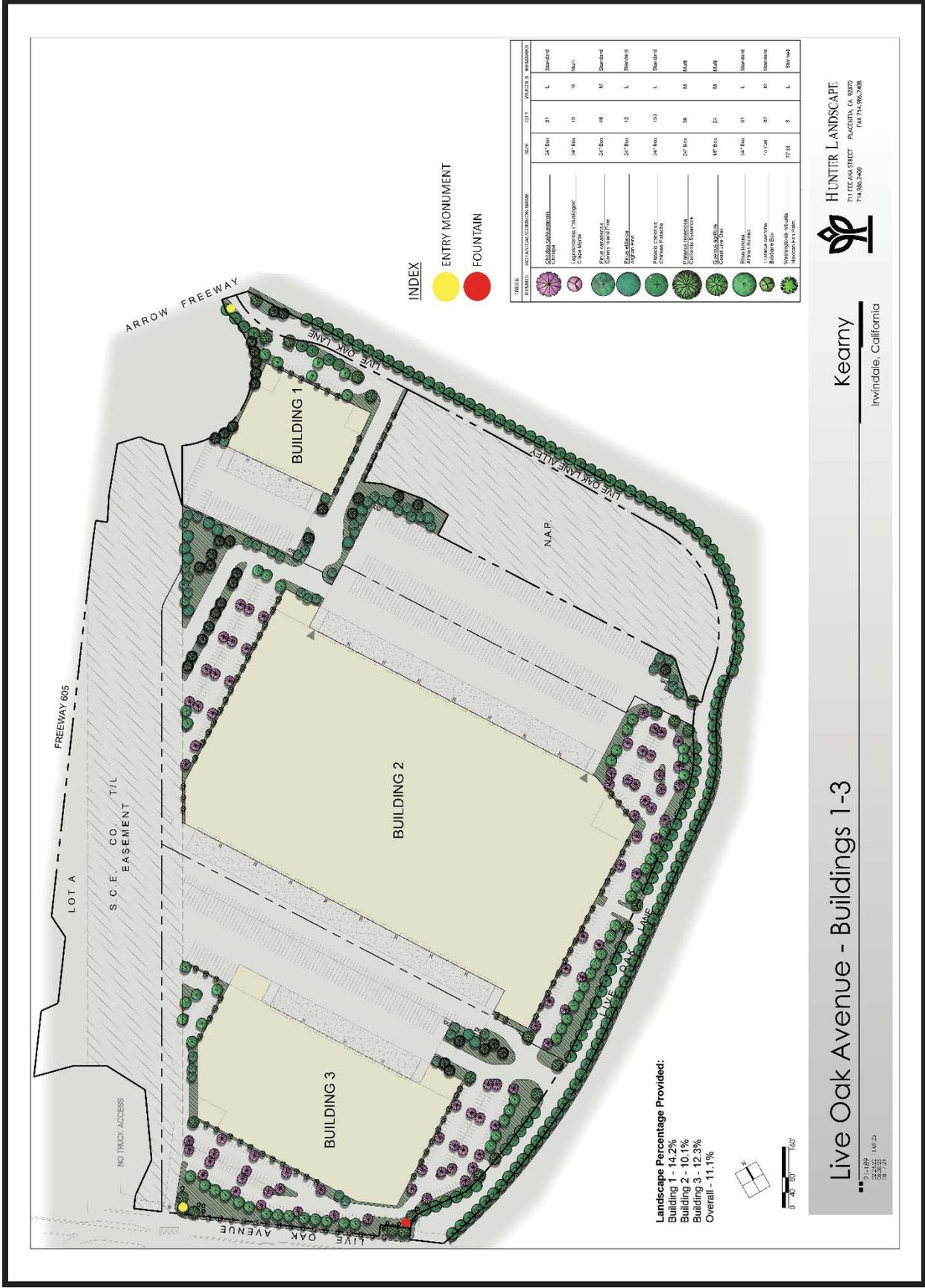


FIGURE 7-7 – CONCEPTUAL LANDSCAPE PLAN WITHOUT BESS

7.5.1 Plant Palette

The plant palette for the Specific Plan includes colorful shrubs and groundcovers, ornamental grasses and succulents, and evergreen deciduous trees – including flowering varieties – that are commonly used throughout southern California, and which complement the Specific Plan’s design theme and setting. The plant materials used within the Specific Plan are water-efficient species that can thrive in the arid southern California climate.

Table 7-1, Plant Palette, provides a list of plant materials approved for use within the Specific Plan. The plants listed in Table 7-1 establish a base palette for the Specific Plan’s landscape design. Other similar plant materials may be substituted for species listed in Table 7-1, provided the alternative plants are drought-tolerant and complement the Specific Plan’s landscape theme.

Table 7-1-Plant Palette

PLANT TYPE	PLANTS
ENTRY ACCENT PALM TREES	Date Palm (<i>Phoenix dactylifera</i>)
STREET TREES	Camphor Tree (<i>Cinnamomum camphora</i>) Carrotwood Tree (<i>Cupaniopsis anacardioides</i>) Jacaranda (<i>Jacaranda mimosifolia</i>) London Plane Tree (<i>Platanus x acerifolia</i>) Ornamental Pear (<i>Pyrus calleryana</i>) Queen Palm (<i>Syagrus romanzoffiana</i>)
PARKING LOT TREES	African Sumac (<i>Searsia lancea</i>) Bottle Tree (<i>Brachychiton populneus</i>) Fern Pine (<i>Podocarpus gracilior</i>) Karee (<i>Rhus lancea</i>)
FLOWERING TREES	Blue Palo Verde (<i>Parkinsonia florida</i>) Western Redbud (<i>Cercis occidentalis</i>) Chinese Flame Tree (<i>Koelreuteria bipinnata</i>) Pink Dawn (<i>Chitalpa tashkentensis</i>) Crape Myrtle (<i>Lagerstroemia</i>)
EVERGREEN TREES	Australian Willow (<i>Geijera parviflora</i>) Brisbane Box (<i>Lophostemon confertus</i>) Coast Live Oak (<i>Quercus agrifolia</i>) Mondell Pine (<i>Pinus eldarica</i>)
SHRUBS	Little John (<i>Callistemon</i>) Coastal Rosemary (<i>Westringia fruticosa</i>) Purple leaf hopbush (<i>Dodonaea viscosa ‘purpurea’</i>) Fortnight Lily (<i>Diets bicolor</i>)

PLANT TYPE	PLANTS
	Indian Hawthorn (<i>Rhaphiolepis indica</i>) Waxleaf Privet (<i>Ligustrum japonicum 'texanum'</i>) Mock Orange (<i>Pittosporum Tobira</i>) Tuscan Blue Rosemary (<i>Rosmarinus o. 'tuscan blue'</i>) Texas Sage (<i>Leucophyllum frutescens</i>)
GROUND COVERS	Desert Carpet (<i>Acacia redolens</i>) Autumn Sage (<i>Salvia greggii</i>) Deer Grass (<i>Muhlenbergia rigens</i>) Small Cape Rush (<i>Chondropetalum tectorum</i>) Dwarf Mat Rush (<i>Lomandra longifolia 'breeze'</i>) Dwarf Yellow Bush Lantana (<i>Lantana m. 'dwarf yellow'</i>) Mexican Sage Bush (<i>Salvia leucantha</i>) Star Jasmine (<i>Trachelospermum jasminoides</i>) Creeping Rosemary (<i>Rosmarinus o. 'prostratus'</i>) Tall Fescue (<i>Festuca arundinacea</i>)

7.5.2 Entry Treatments and Project Identification

Project Monuments

Three (3) project monuments (signs and water features) are provided to identify the Development Area. A monument sign fronting Live Oak Avenue across from Graham Avenue will provide identification on the south end of the Development Area (see Figures 7-8 and 7-9). This monument will be highly visible from the I-605 Live Oak offramp. A second monument sign at the project entry on Live Oak Lane at the intersection with Arrow Highway will provide identification on the north end of the Development Area (see Figures 7-10 and 7-11). A fountain at Live Oak Lane and Live Oak Avenue will provide an attractive welcome as motorists and pedestrians enter the Development Area (See Figure 7-12). The entry treatments are designed to provide distinctive visual statements and emphasize the Development Area's contemporary aesthetic.

The project monuments described and illustrated herein are designed to provide a strong sense of arrival to employees, visitors, and passing motorist, to identify the distinctiveness of the Development Area, and to complement and reinforce the Development Area's general architectural and landscape theme. Implemented entry treatments may differ slightly from the concepts presented herein; however, all corner entry treatments provided within the Development Area shall be consistent in theme and character. The designs of these entry treatments are conceptually shown below. Each location provides a monument sign and landscaping consisting of water features, accent palm trees, drought tolerant ground cover and shrub masses, screen shrubs, and street trees. Flowering and colorful plant material is recommended.

Building Site Entry Treatments

Entry treatments for building sites may be provided at driveways connecting to Live Oak Avenue or Live Oak Lane. The locations of such driveways will be determined at the time Buildings are designed and oriented in the Development Area as part of implementing development projects. Building entry treatments are meant to identify Building occupants and welcome employees and visitors to the site. The designs of typical Building entry treatments are conceptually shown below, and may include signs, flowering accent trees, drought tolerant groundcover and shrub masses, evergreen screen trees, and enhanced Building entry paving.

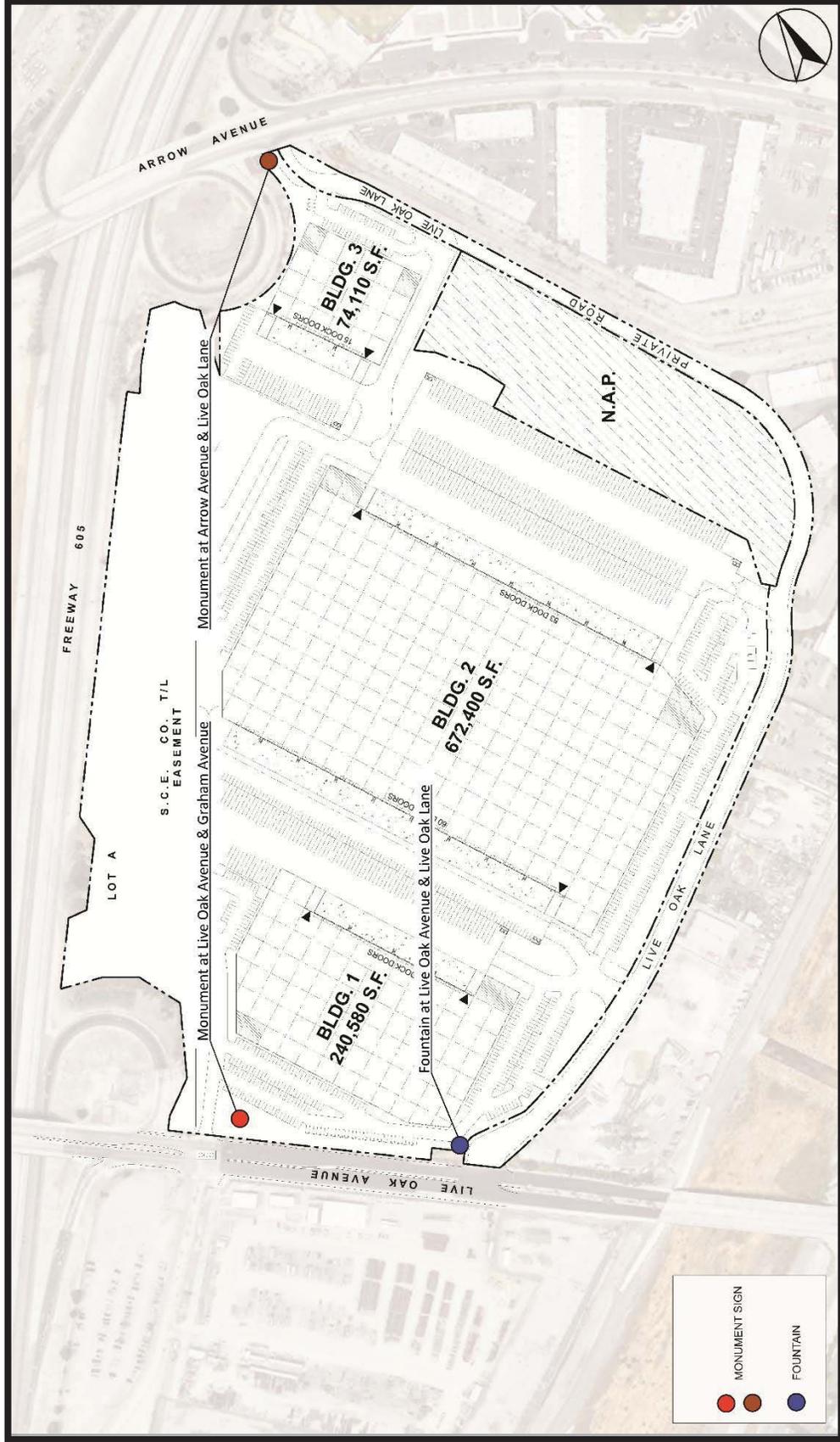


FIGURE 7-8 – ENTRY TREATMENT LOCATION MAP



FIGURE 7-9 – ENTRY TREATMENT - LIVE OAK AVENUE AT GRAHAM AVENUE

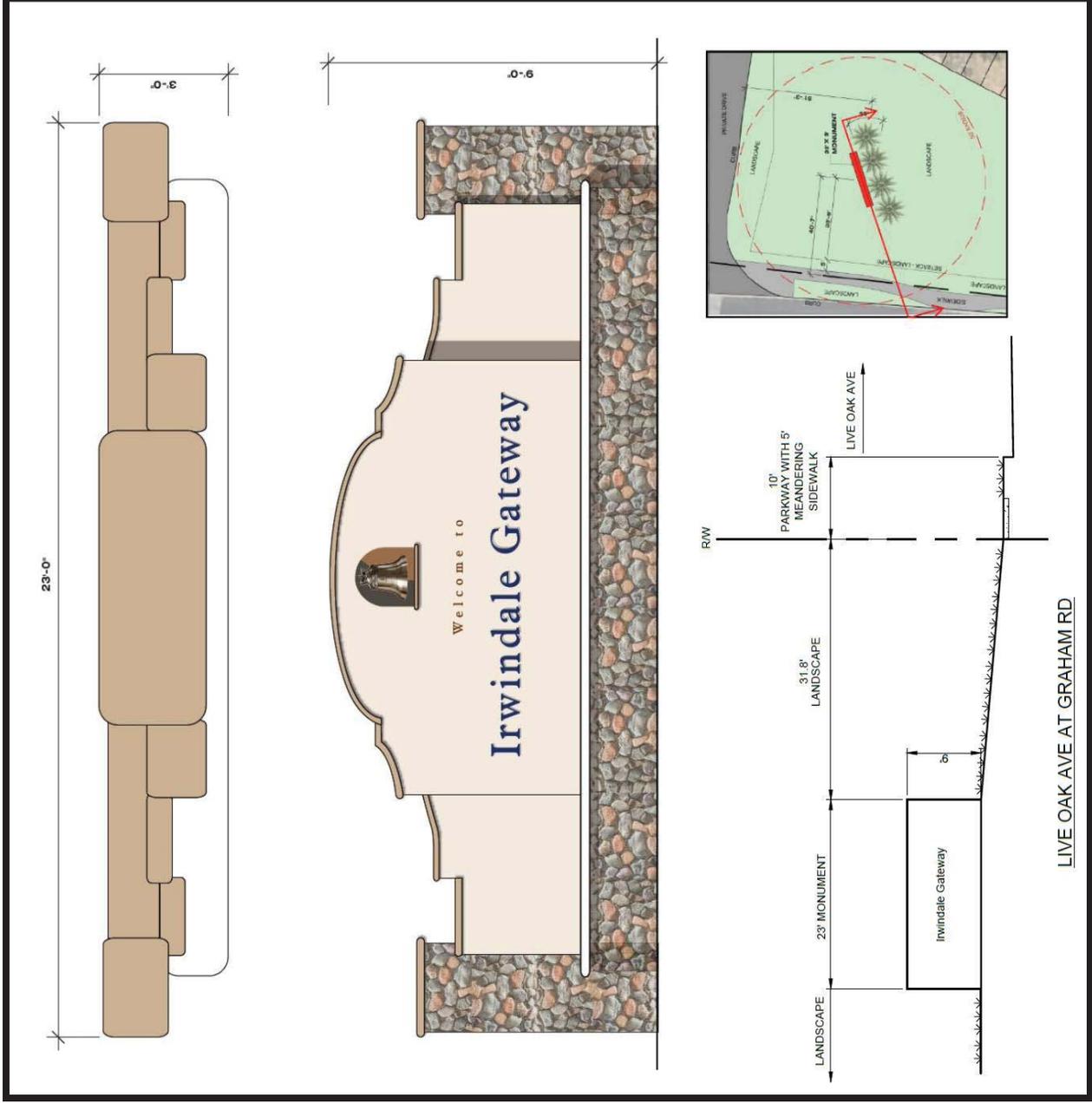


FIGURE 7-10 – ENTRY TREATMENT - LIVE OAK AVENUE AT GRAHAM AVENUE (DETAIL)



FIGURE 7-11 – ENTRY TREATMENT – ARROW HIGHWAY AT LIVE OAK LANE

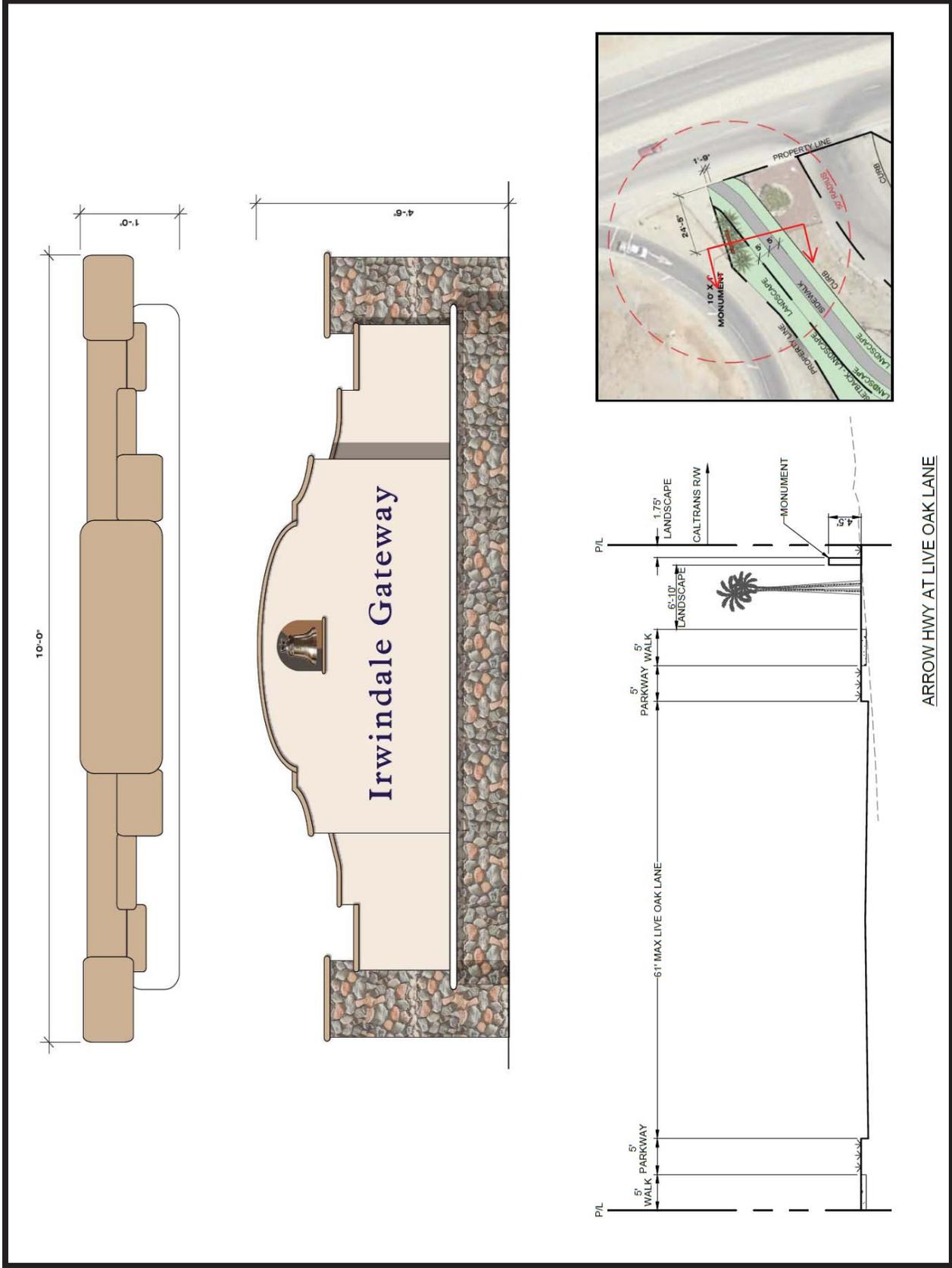


FIGURE 7-12 – ENTRY TREATMENT – ARROW HIGHWAY AT LIVE OAK LANE (DETAIL)



FIGURE 7-13 – ENTRY TREATMENT LIVE OAK AVENUE AT LIVE OAK LANE

7.5.3 Streetscapes

Streetscape landscaping within the Specific Plan plays a critical role in establishing a strong sense of place and character. In addition, streetscapes serve functional purposes, including screening undesirable functional elements of a building site from public view.

Live Oak Avenue Streetscape

The southern boundary of the Specific Plan abuts Live Oak Avenue. The interface with Live Oak Avenue is designed to feature a five-foot wide parkway with street trees and a five-foot wide sidewalk between the parkway and property line (see Figure 7-13). A backdrop of trees along parking lot perimeters, and screen shrubs along with assorted drought tolerant groundcovers is proposed to provide a physical and visual buffer from Live Oak Avenue. At this interface, an assortment of evergreen and deciduous canopy street trees along with palm tree clusters and screen shrubs is expected.

Live Oak Lane Streetscape

The eastern boundary of the Specific Plan abuts Live Oak Avenue. Within the Specific Plan, the streetscape of Live Oak Lane is planted with a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists (see Figure 7-14). A five-foot wide parkway with street trees and a five-foot wide sidewalk between the parkway and property line is also proposed. The landscaping plant palette for the streetscape should link the roadway to the rest of the Development Area and should reflect the Development Area's landscape design theme.

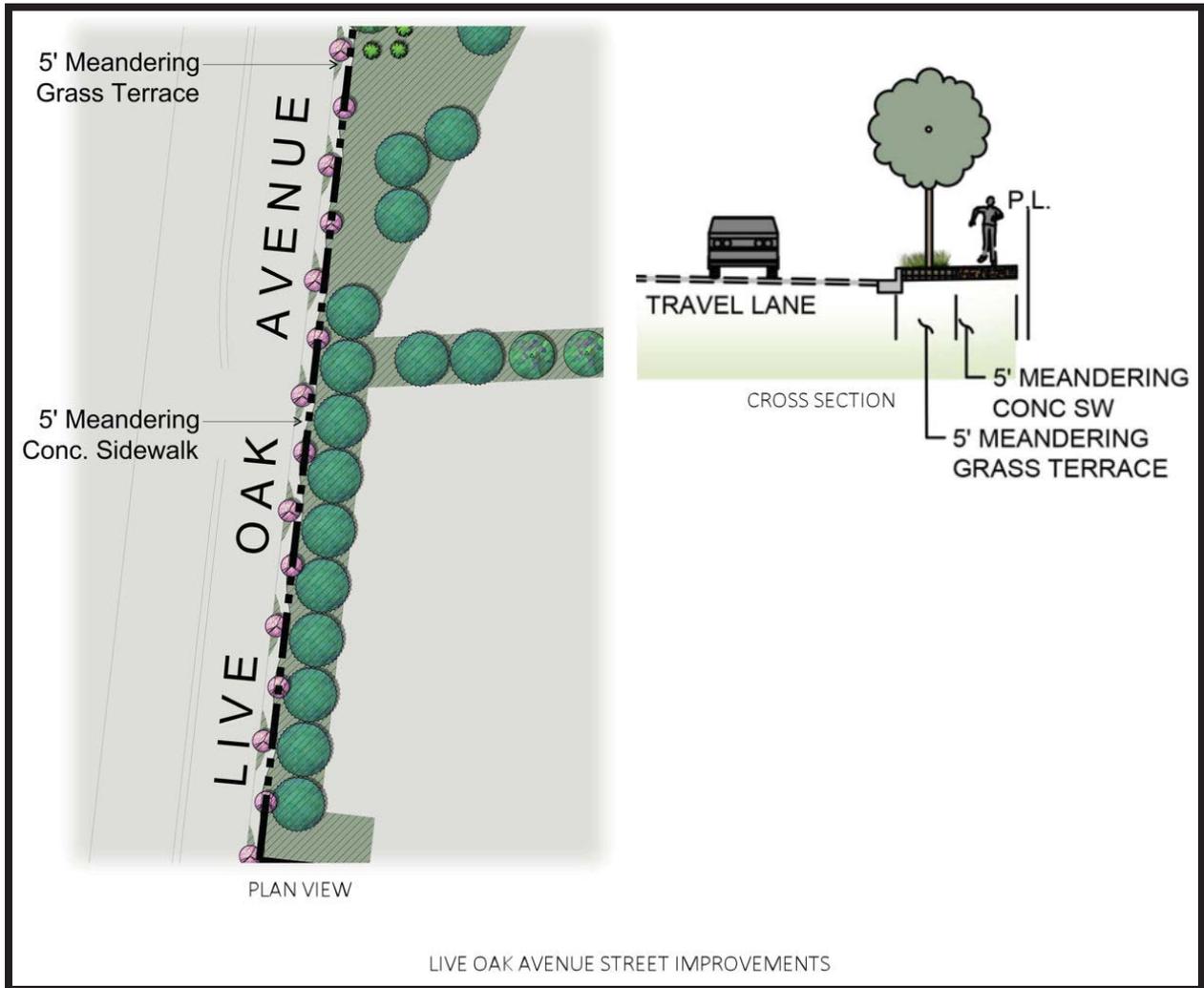


FIGURE 7-15 – STREET IMPROVEMENTS – LIVE OAK AVENUE

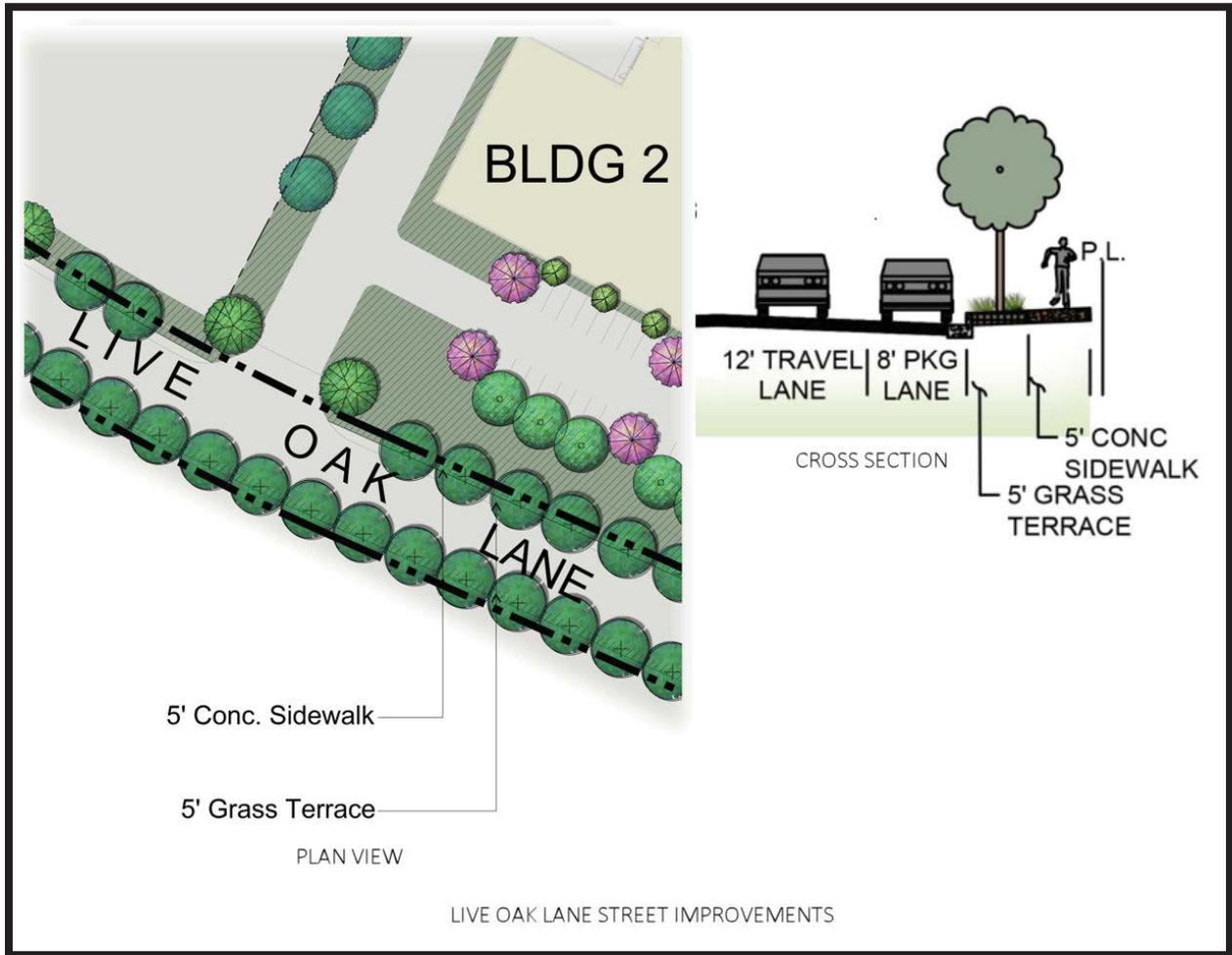


FIGURE 7-16 – STREET IMPROVEMENTS – LIVE OAK LANE

7.6 Fences and Walls

The final locations and details of walls and fences will be determined during implementation of the Specific Plan when building footprints and orientations are known. Implementing projects may modify the locations and/or details of the walls and fences described below and illustrated in Figures 7-15, 7-16, and 7-17, provided that any modifications are consistent with the spirit and intent of these guidelines.

The existing 6-foot-tall chain link fence along the western border of the Specific Plan abutting I-605 will remain in place. Tubular steel fencing is expected within individual building sites around loading and dock areas, truck yards, and surface detention basins. Tubular steel fencing is designed to range from 4 to 10 feet in height, consisting of tubular steel pickets.

Solid screen walls may be provided within individual building sites around loading and dock areas, truck yards, and parking lots. A solid wall is preferred over fencing when complete visual screening is necessary, or for noise attenuation. Screen walls are designed to be a height of 14 feet tall, as measured from the high side of the wall. Screen walls may be constructed of a decorative masonry, decorative CMU block, or concrete panel; provided, however, that a screen wall of decorative CMU block approximately 620' long shall be located along the southern boundary of the Development Area to screen the area from Live Oak Avenue. Landscaping, including trees, should be planted along the outer face of the screen wall (i.e., facing Live Oak Avenue or Live Oak Lane) to minimize the perceived size and scale of the wall.

The locations and details of walls and fences internal to the Development Area will be determined in conjunction with development of each site. Internal walls and fences may be provided along the perimeter of parking and loading areas and between building pad areas for screening and security. All internal walls visible from public streets or public viewing areas shall be built with attractive, durable materials that are compatible with other design elements of the Specific Plan.

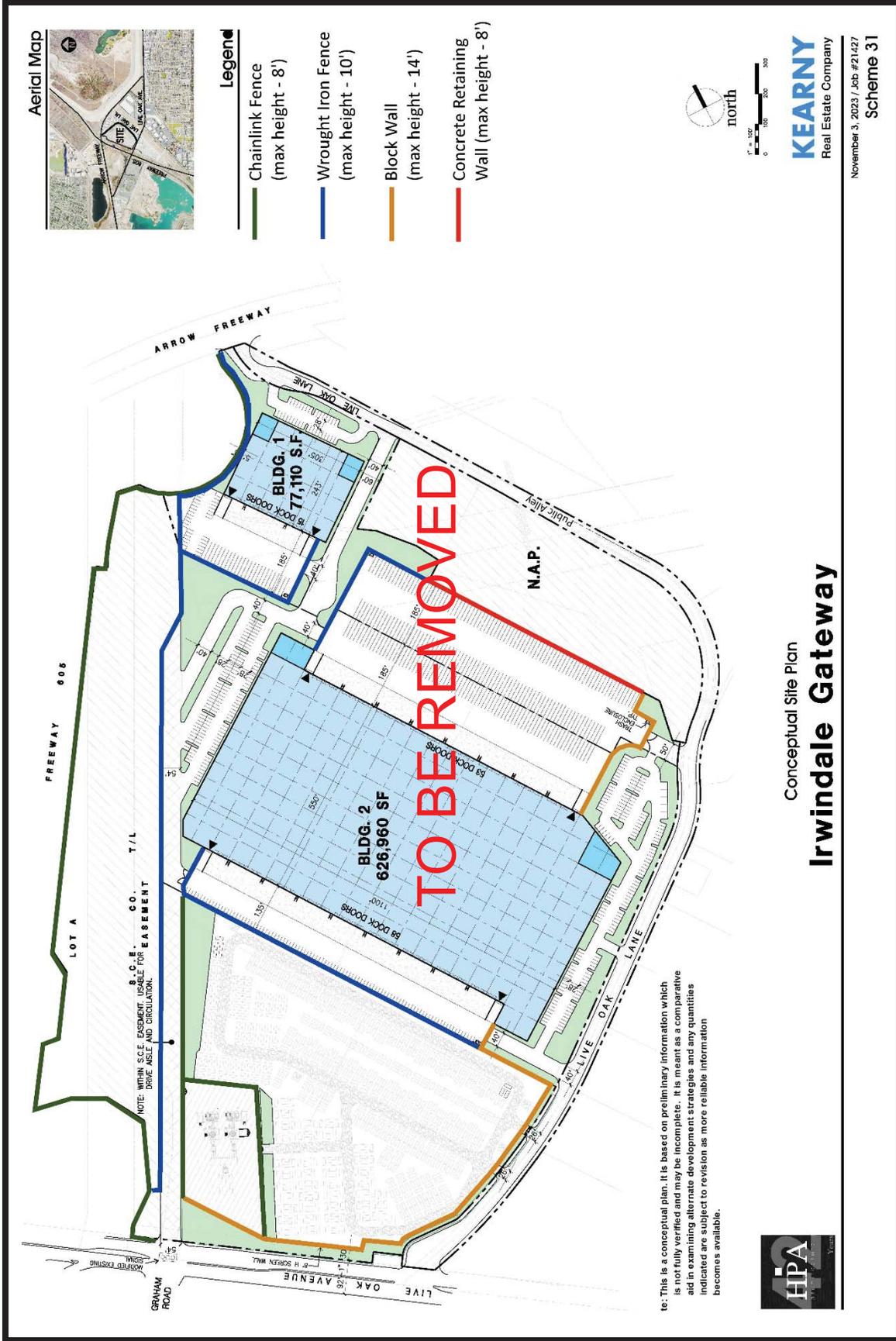


FIGURE 7-17 — FENCE AND WALL PLAN WITH BESS

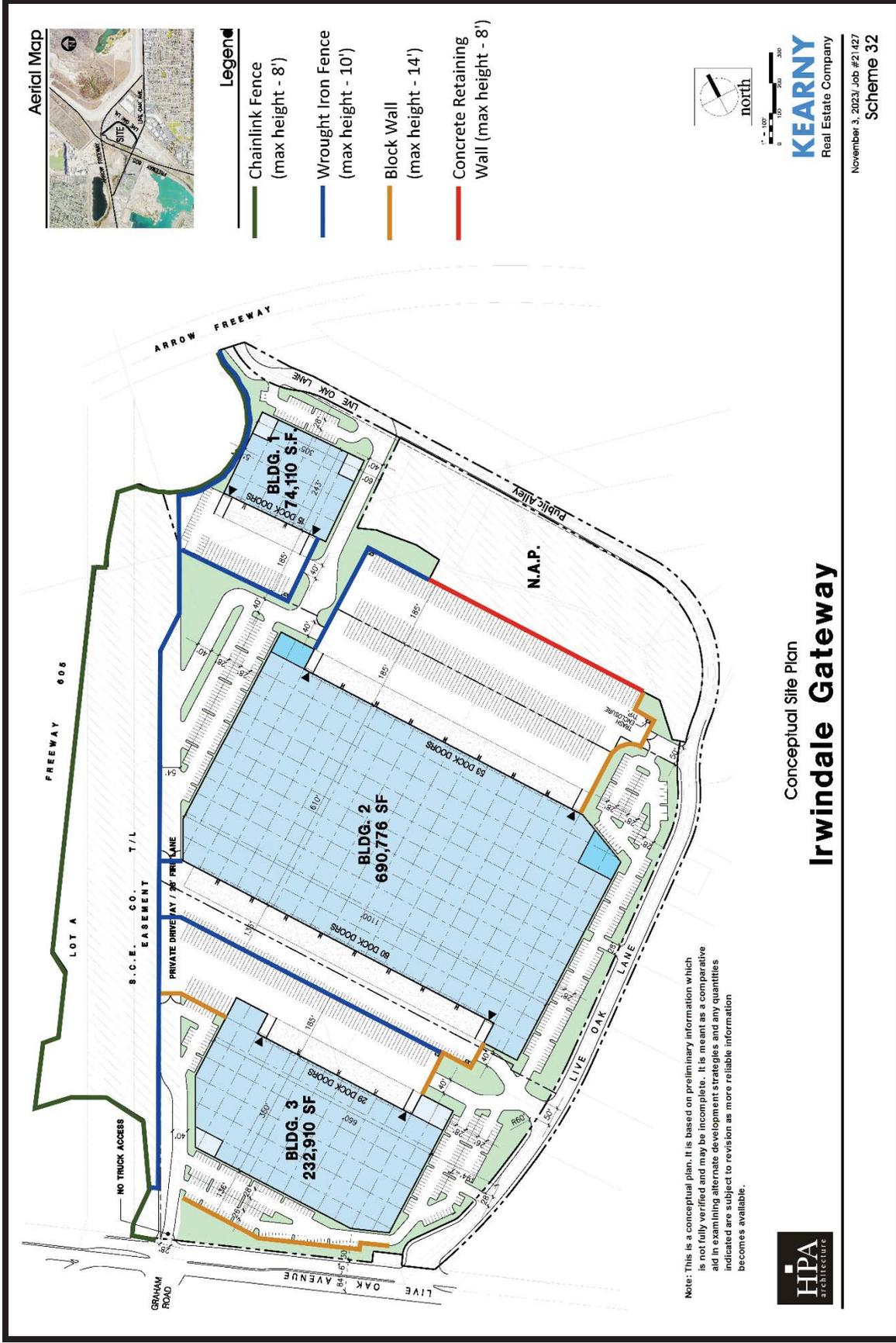


FIGURE 7-18 – FENCE AND WALL PLAN **WITHOUT BESS**

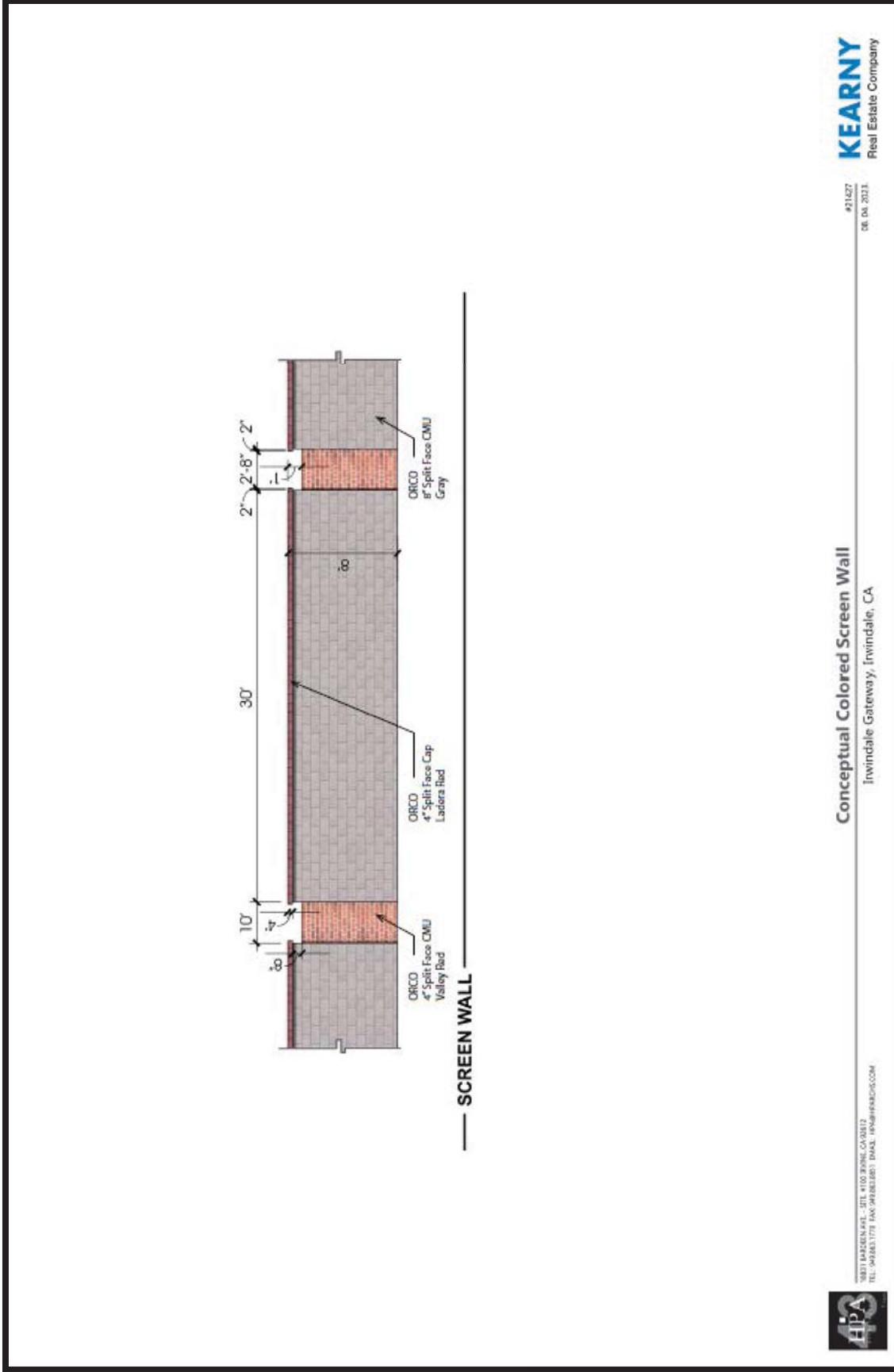


FIGURE 7-19 –WALL PLAN DESIGN

7.7 Lighting

Outdoor lighting of the Development Area is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting within the public right-of-way shall adhere to applicable City of Irwindale requirements.

All lighting on private property in the Development Area should adhere to the following guidelines:

1. Minimize glare and “spill over” light onto public streets and adjacent properties by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, parking, loading, unloading, and similar areas. Where desired, illuminate trees and other landscape features by concealed light fixtures. Limit light spillover or trespass to one-quarter foot-candle or less, measured from within five feet of any adjacent property line.
2. Exterior lighting shall produce a maximum initial illuminance of no greater than 0.5 horizontal and vertical footcandles at the site boundary and beyond.
3. The maximum height of free-standing, outdoor light fixtures shall be 35 feet. The maximum height for outdoor bollard-type lighting shall be four (4) feet. Overall, light fixtures shall be the minimum height necessary to maintain pedestrian and motorist safety and facilitate site operations and security.
4. Select all lighting fixtures used in the Development Area from the same – or complementary – family of fixtures with respect to design, materials, fixture color, and light color.
5. Lights should be unbreakable, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
6. Neon, flashing and gyrating lighting are prohibited.
7. Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.
8. Illuminate parking lots, loading dock areas, pedestrian walkways, Building entrances, and public sidewalks to the level necessary for Building operation and security reasons. Dimmers and motion detectors are permitted. Lighting shall have automatic shut off features between dawn and dusk.
9. Use exterior lights to accent entrances, plazas, activity areas, and special features.
10. Provide for illumination intensity during hours of darkness as follows:
 - a. To illuminate parking lots or parking structures and their pedestrian links that provide more than five parking spaces for use by the general public, provide a minimum coverage of one foot-candle of light with a maximum of eight foot-candles on the parking or walkway surface, unless otherwise approved by the City of Irwindale for visibility and security.
 - b. To illuminate aisles and passageways within a Building complex, provide a maximum of one-half to one foot-candle of maintained lighting.
11. Low intensity, energy-conserving night lighting is preferred, such as fixtures equipped with light emitting diodes (LED).
12. High Pressure Sodium (HPS) light fixtures are prohibited for site lighting.

Utility Infrastructure Plan

VIII

8. Utility Infrastructure

Buildout of the Specific Plan requires the installation of water, sewer, storm water drainage dry utility infrastructure, and solid waste disposal as described below. All utility infrastructure improvements are required to be installed in accordance with applicable City of Irwindale and public service provider design standards and specifications.

8.1 Water

As shown on Figures 8-1 and 8-2, Conceptual Water Plan, Valley County Water District provides water infrastructure to service the Specific Plan area. An existing 12" Valley County Water District main is located in Live Oak Lane, a private road. To service the Development Area with domestic water, including fire protection service and irrigated landscaping, a connection would be made to the existing line to service future buildings and facilities. Water and fire service would be provided with a new 12-inch main comprising private on-site loop system that connects to the existing water line in Live Oak Lane. Alternatively, the water district may require new infrastructure to loop its system through Live Oak Lane. All water service and connection to the distribution system shall be reviewed and approved by the Valley County Water District.

8.2 Sanitary Sewer

As shown on Figures 8-3 and 8-4, Conceptual Sewer Plan, the Specific Plan provides private sewer infrastructure for the interior of the Development Area. Connections to existing public sewer mains are located off-site within Live Oak Avenue to the southeast of the Development Area. The sanitary sewer system for the Development Area will utilize 6"-8" on site private sewer lines, which will be conveyed to a proposed public 6" force main sewer line that drains east on Live Oak Avenue to the existing 10" VCP sewer line on Live Oak Avenue and Rivergrade Road. The private sewer infrastructure collects wastewater flows from the Development Area and conveys these flows to the off-site existing public sewer mains within Live Oak Avenue. All private sewer infrastructure will be installed on-site beneath private driveways and drive aisles, and/or parking lots/truck courts to facilitate access for routine maintenance and/or repair. Locations and alignments of all sewer mains, laterals and connection points shall be subject to the approval of the City Engineer from the City of Irwindale Public Works Department.

8.3 Storm Water Drainage

Prior to use of the Development Area property as a quarry, stormwater flowed across the site from the north and east to the southwest and would leave the site at its southwest corner and discharge to an existing unimproved drainage basin located on the western portion of the Specific Plan within an SCE easement area. As shown on Figures 8-5 and 8-6, Conceptual Storm Water Management Plan the Specific Plan's storm water management system will mimic the property's historical drainage pattern. Storm water flows will be conveyed across the Development Area via a backbone storm drain network to a detention/infiltration basin, which will be constructed pursuant to the approved LID and will replace the existing unimproved drainage basin. A storm drain lift station will convey water southeast through a 12" private force main and into a proposed 24" public storm drain within Live Oak Avenue, and then into the San Gabriel River.

Low Impact Development (LID) site design strategies and Best Management Practice (BMP) control measures promote the use of natural infiltration (where permitted), evaporation, and use of stormwater. LID strategies include, in order of priority: 1) bioretention/infiltration systems, 2) capture and reuse systems, 3) high efficiency biofiltration systems, and 4) proprietary filtration systems. The primary LID measure for the Development Area includes detention basins, capable of retaining the required water quality volumes, designed with either soft bottoms and/or dry wells for infiltration purposes as water quality measures. Where feasible, subsurface storage chambers that serve as water quality infiltration measures can be implemented within the Development Area. The type and extent of the water quality infiltration measures will ultimately be determined based on geotechnical report findings and recommendations for the Development Area. If infiltration is restricted or not feasible in some areas, based on the geotechnical report findings, then capture and reuse, bio-filtration and/or inlet filters as water quality measures are allowed as treatment control BMPs within the Development Area. All LID and BMP features shall comply with the City of Irwindale Building Code and will require grading and drainage permits from the Building & Safety Division.

8.4 Dry Utilities

As shown on Figures 8-7 and 8-8, Dry Utilities, the Development Area will connect to existing dry utilities (electric, gas, and communication systems) installed within Live Oak Avenue. All dry utilities internal to the Development Area will be installed underground in utility trenches. The locations of trenches, lateral connections, transformers, switches, pull boxes, and dry utility manholes will be determined at the time Buildings are positioned in conjunction with implementing development.

~~A BESS facility may include an onsite "collector" substation that would connect via a 230 kV underground electric tie-line to a Point of Interconnection (POI) at the existing Southern California Edison (SCE) Edison Rio Hondo Substation located south of Live Oak Avenue. The electrical tie-line shall be undergrounded unless the applicable agency(ies), including but not limited to Southern California Edison or California Independent System Operator (CALISO), reject the proposed underground line. If the applicable agency(ies) require an overhead tie-line, the Applicant shall be subject to a Zone Variance application per IMC Chapter 17.32. The overhead line would transition to underground at a transition pole. The underground portion of the electric tie-line would consist of conduits containing electric power cables, fiber optic communications cable, and a grounding conductor within an approximately 3' 0" wide and 3' 0" deep, high-strength concrete encasement that would be a minimum of 3' 0" below the surface. Impacts related to the undergrounding of the electric tie-line would not result in a net increase of impacts of the proposed projects.~~

8.5 Solid Waste Disposal

The City currently contracts with Athens Services to provide businesses with a full spectrum of solid waste disposal that includes routine trash removal, recyclable collection, organic waste collection, bulky item removal (upon request only), and hazardous waste removal. And a restaurant food waste composting pilot program. Accordingly, all refuse collection services will be available to the development from Athens Services. The development shall provide trash enclosures to accommodate the size, type, and number of bins required for the uses on site.

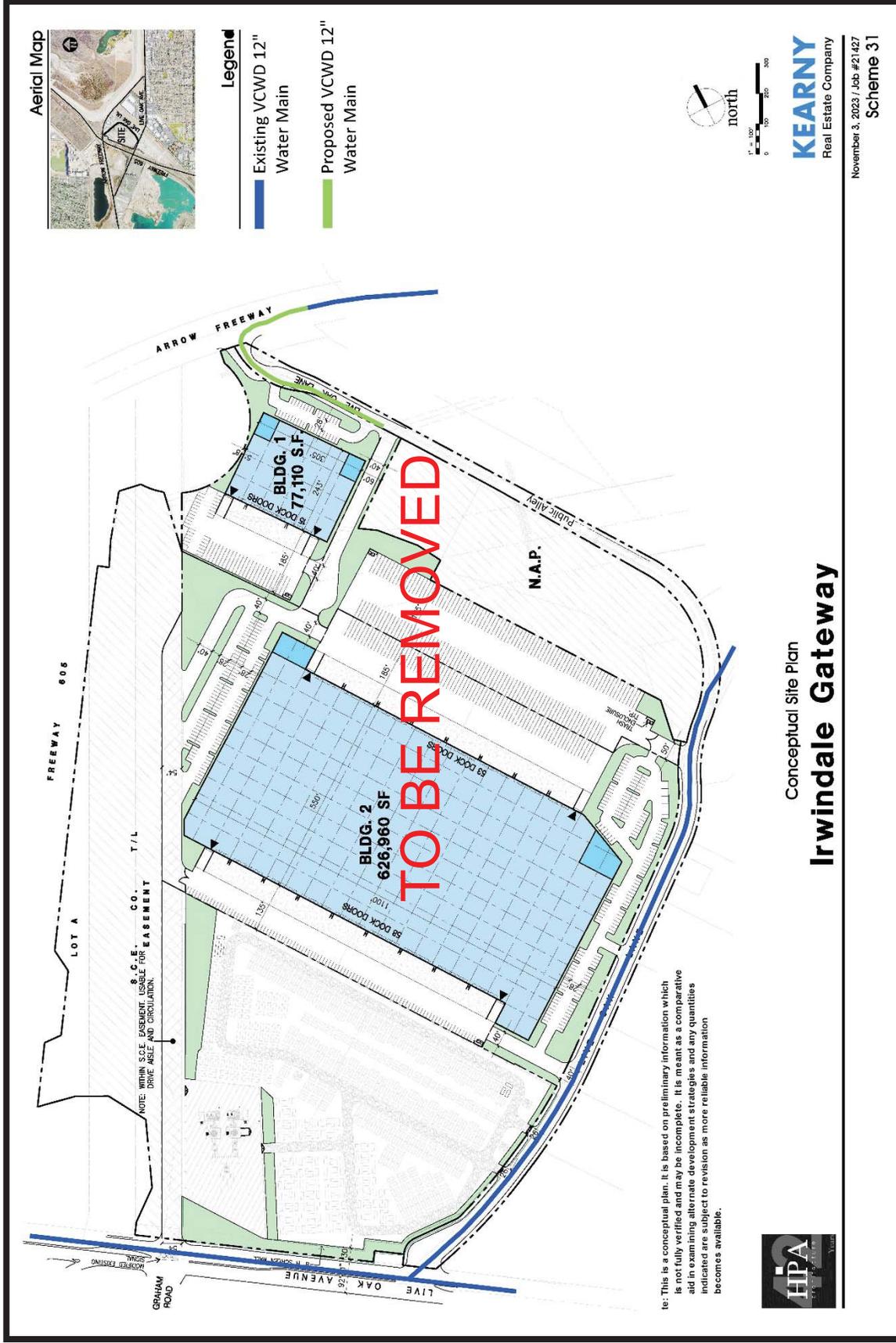


FIGURE 8-1 -- CONCEPTUAL WATER PLAN WITH BESS

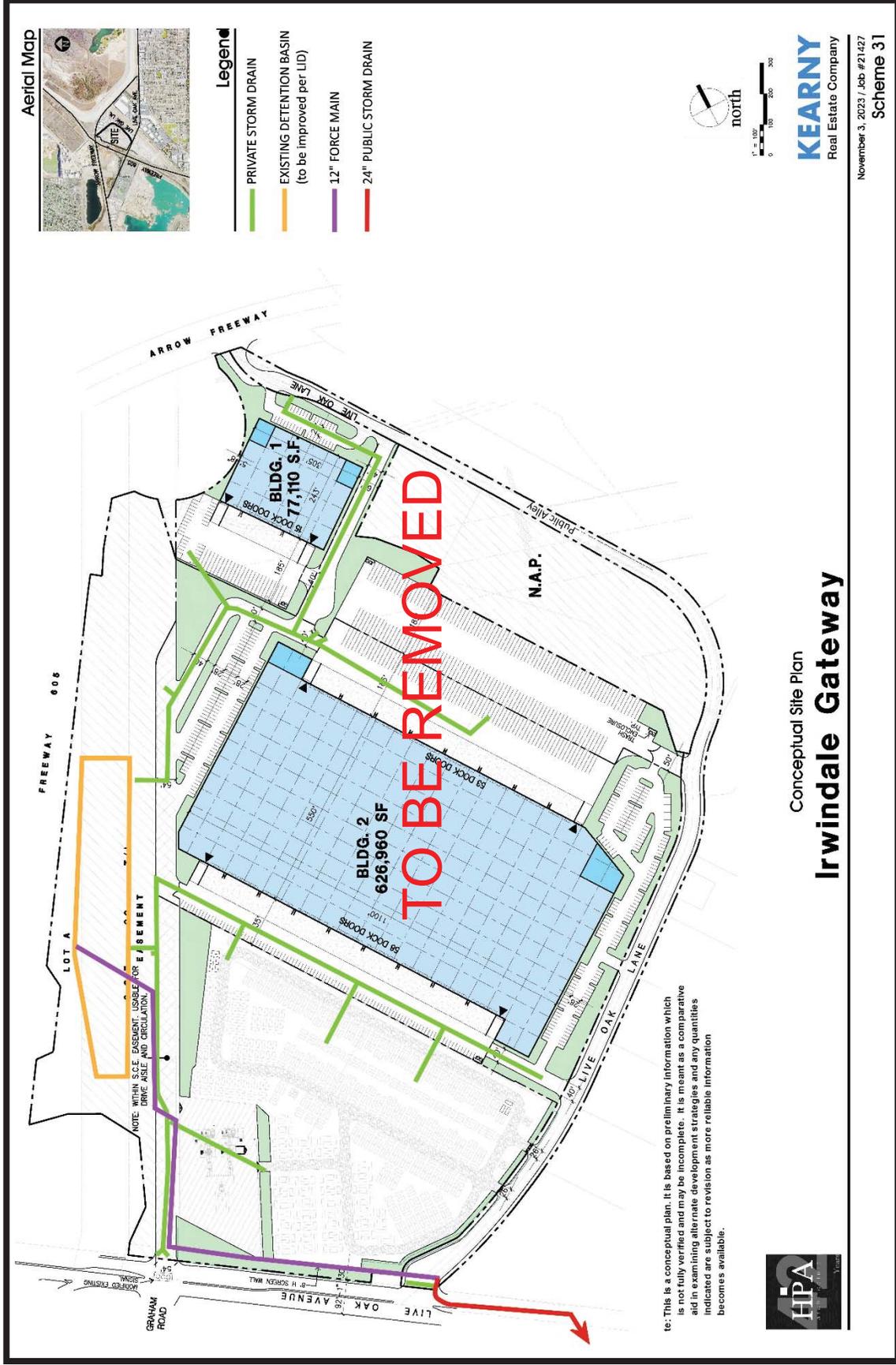


FIGURE 8-5—CONCEPTUAL STORM-WATER MANGEMENT PLAN WITH BESS

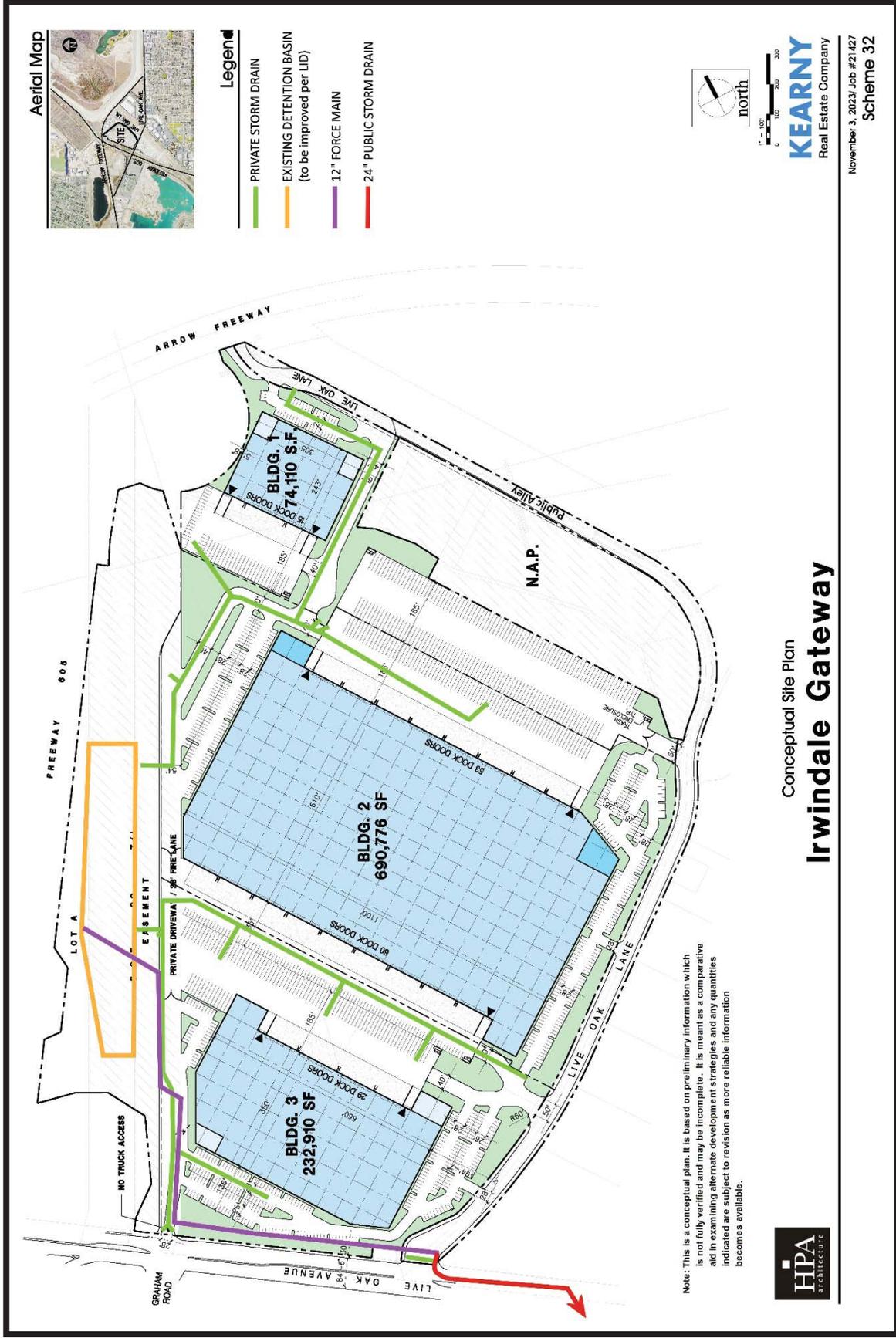


FIGURE 8-6 – CONCEPTUAL STORM WATER MANGEMENT PLAN ~~WITHOUT BESS~~

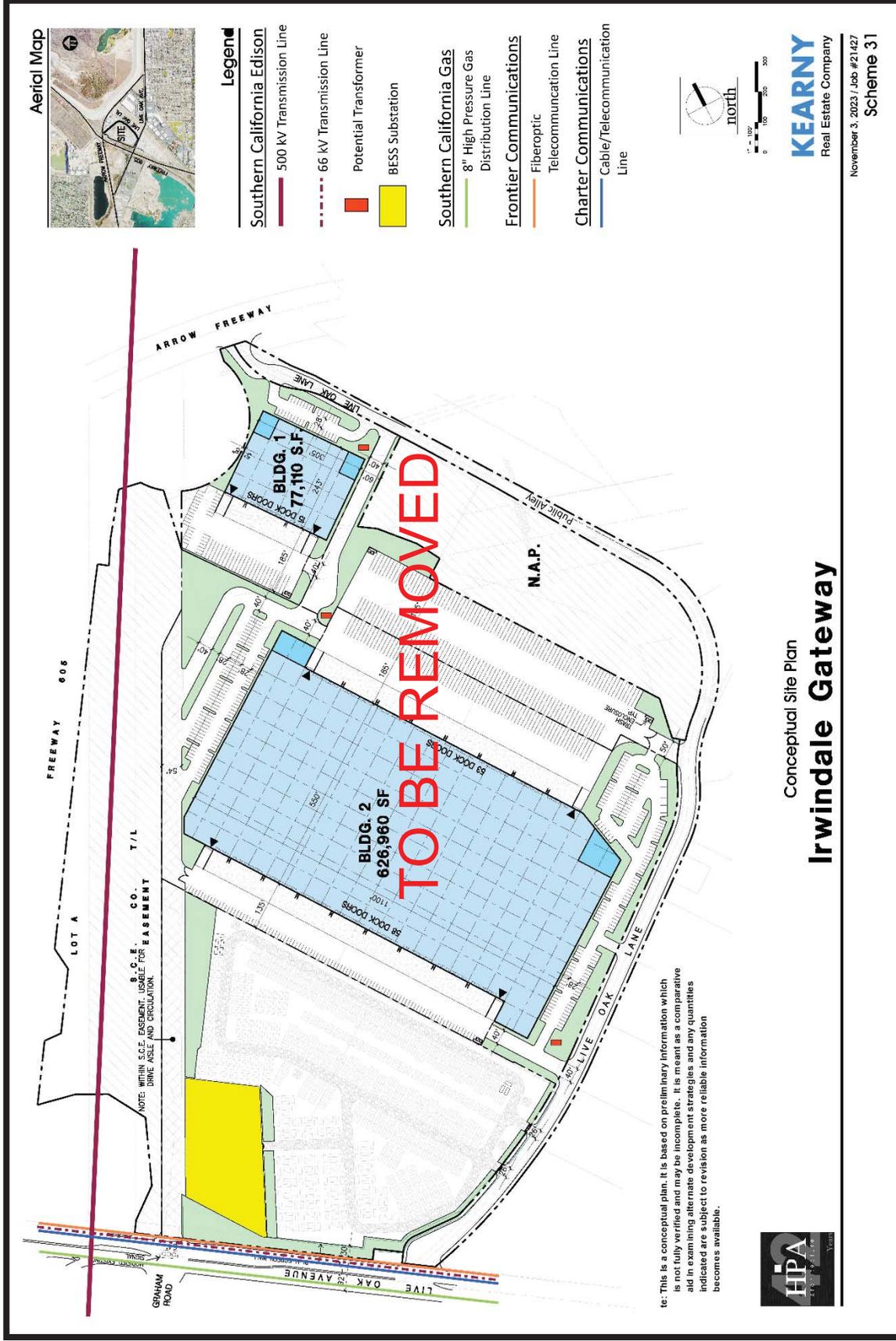


FIGURE 8-7—CONCEPTUAL DRY UTILITIES PLAN WITH BESS

IMPLEMENTATION

IX.

9. Implementation

9.1 Overview

This section establishes the implementation and review process required for development proposed within the Specific Plan Area. This section provides general administrative provisions; review and approval procedures; and implementation measures, including short-term and ongoing tasks. The provisions contained in the Specific Plan constitute the primary land use and development standards for the Development Area. While the entire Specific Plan constitutes the zoning for the Specific Plan area, Section 6, Development Standards, contains specific zoning regulations for the Development Area. Upon adoption of the Specific Plan by the City of Irwindale, all on- and off-site improvements shall be consistent with the development standards and design guidelines set forth in Sections 6, 7, and 8 of this Specific Plan.

Implementation of development within the Development Area shall be subject to City approval of lot line adjustments, site plan review, plot plans, subdivision reviews, building permits, and other planning approvals and permits that may be required by the City of Irwindale pursuant to the Specific Plan or the City of Irwindale Municipal Code. The implementation process described herein provides the procedures for review and approval of development within the Specific Plan Area.

9.2 Severability

If any portion of this Specific Plan is declared to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions hereof. The legislative body hereby declares that they would have enacted these regulations and each portion thereof irrespective of the fact that any one or more portions be declared invalid or ineffective.

9.3 Interpretations and Determinations

Requests for interpretations of this Specific Plan and verifications relating to prior approvals or permits may be made to the Community Development Director. Requests shall be in writing. The decision of the Community Development Director on such requests may be appealed pursuant to Chapter 17.25 (Common Procedures) of the IMC.

9.4 Site Plan and Design Review

All development within the Development Area shall be subject to Site Plan and Design Review, as set in the Chapter 17.27 (Site Plan and Design Review) of the IMC.

9.5 Conditional Use Permits

Conditional Use Permits are provided for the individual review of uses at specific locations, as outlined in Table 6-1 to ensure that their operation will be compatible with surrounding areas and uses. Applications for Conditional Use Permits shall be reviewed pursuant to Chapter 17.28 (Use Permits) of the IMC.

9.6 Minor Variance

As part of a Site Plan and Design Review application, the Community Development Director or Planning Commission, as applicable, may grant a minor variance to the development standards listed in the Specific Plan, pursuant to Chapter 17.31 (Minor Variances) of the IMC.

9.7 Amendments to the Specific Plan

All modifications to the Specific Plan that do not meet the criteria of a minor modification (see Section 9.6) or any applicable interpretation made pursuant to Section 9.3 shall require an amendment to the Specific Plan Amendments shall be processed in accordance with § 65453, et. seq. of the California Government Code, which requires Specific Plan Amendments be reviewed and adopted in the same manner as a General Plan and Chapter 17.35 of the IMC.

9.8 Subdivision

If a proposed project requires a subdivision, the subdivision shall be consistent with and serve to implement the policies and provisions of the Specific Plan and all applicable City policies and ordinances as required by Title 16 (Subdivisions) of the IMC. All subdivisions shall be authorized through the approval of a map or other approval in compliance with Title 16 (Subdivisions) of the IMC and the California Subdivision Map Act for Land.

9.9 Financing of Development in Development Area

Development in the Specific Plan Area and any off-site improvements to roadways or infrastructure required as conditions of approval or mitigation measures identified as part of the Environmental Impact Report prepared for the project pursuant to CEQA and adopted by the City shall be privately funded. All development shall be subject to Development Impact Fees and any other applicable fees.

9.10 Maintenance Plan

The public and private improvements constructed within the Specific Plan Area shall be maintained through a combination of public and private entities as described in Table 9-2.

Major infrastructure costs may be offset by public assistance such as a Community Facility District (CFD) or other special districts to provide funding for the construction of a variety of public facilities and the provision of public services. City Council approval is a prerequisite for the implementation of all special district-financing mechanisms.

For common areas located within the Development Area, a Private Maintenance Association(s) shall be established with recorded covenants, conditions and restrictions to govern allocation of maintenance responsibilities among property owners. Maintenance of public facilities shall be the responsibility of the appropriate public agency.

TABLE 9-2 | MAINTENANCE RESPONSIBILITIES

FACILITY	PRIVATE MAINTENANCE ASSOCIATION	CITY OF IRWINDALE	PUBLIC UTILITY	OTHER MAINTENANCE ENTITY ¹
CIRCULATION & RELATED FACILITIES				
Live Oak Avenue				
Pavement & Curbs		X		
Landscaping within public right-of-way, including medians and parkways		X		
Sidewalks		X		
Pavement & Curbs		X		
Landscaping within public right-of-way, including medians and parkways		X		
Sidewalks		X		
Live Oak Lane				
Public Alley		X		
Private Drive Aisles	X			
Parking Lots, including landscaping	X			
Traffic Signals ³		X		
Traffic Signs				
Within public right-of-way		X		
Within private property	X			
Street Lights				
Within public right-of-way			X	
Within private property	X			
Truck Traffic Management Plan Directional Signage	X			
LANDSCAPING & RELATED FACILITIES				
Common area landscaping, including entry treatments at Live Oak Avenue and Arrow Highway	X			
Monuments and Signage	X			
Walls and Fences	X			
UTILITY INFRASTRUCTURE				
On-site water facilities/infrastructure	X		X	
On-site sanitary sewer facilities/infrastructure	X	X		
Private storm water drainage facilities/infrastructure	X			
Public storm water drainage facilities/infrastructure		X		X
Dry utilities (electricity, natural gas, communications systems)			X	X

TABLE 9-2 MAINTENANCE RESPONSIBILITIES				
FACILITY	PRIVATE MAINTENANCE ASSOCIATION	CITY OF IRWINDALE	PUBLIC UTILITY	OTHER MAINTENANCE ENTITY ¹
<p>Notes:</p> <ol style="list-style-type: none"> 1. Other Maintenance Entities may include Caltrans, utility providers, and other public/private entities that may maintain billboards and stormwater drainage facilities on-site. 2. Traffic signals may require maintenance easements for the City to access and maintain the traffic signals. 				

9.11 California Environmental Quality Act

An Environmental Impact Report (the “EIR”) prepared in compliance with the requirements of the California Environmental Quality Act (“CEQA”) for the Specific Plan identified environmental impacts associated with the project requiring mitigation.

The EIR identifies potential impacts resulting from development and establishes mitigation measures to reduce the impacts to a less than significant level. As the Lead Agency, the City of Irwindale has adopted a Mitigation Monitoring and Reporting Program (MMRP) as part of this Specific Plan.

The EIR will serve as the primary environmental clearance document for the Specific Plan and all future development undertaken within the Specific Plan Area. The EIR is considered the primary environmental clearance document for the implementation of the project, including infrastructure, roadway, and any other related on-/off-site improvements. Development applications that require discretionary review will be examined in consultation with the EIR to determine if additional environmental documentation is required. No further analysis would be conducted on projects determined to be exempt from CEQA or in full compliance with the adopted EIR. However, the project applicant will be required to submit documentation evidencing said development is allowed and in conformance with the Specific Plan and that the potential environmental effects are within the parameters analyzed within the EIR.

ATTACHMENT “B”

RESOLUTION NO. 2025-01-3589

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IRWINDALE (1) CERTIFYING THE ENVIRONMENTAL IMPACT REPORT, STATE CLEARING HOUSE (SCH# 2023020290); (2) ADOPTING THE FINDINGS REQUIRED BY CEQA GUIDELINES, SECTION 15091; (3) ADOPTING THE PROPOSED MITIGATION MONITORING AND REPORTING PROGRAM; AND (4) ADOPTING A STATEMENT OF OVERRIDING CONSIDERATIONS, PURSUANT TO THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) FOR THE IRWINDALE GATEWAY SPECIFIC PLAN

A. RECITALS.

- (i) Jeff Dritley, on behalf of KP Irwindale Owner, LLC 1875 Century Park East, Suite 380, Los Angeles, CA 90067, the Applicant, has made the following requests: 1) General Plan Amendment (GPA) No. 02-2022 to change the Land Use Designation from Regional Commercial to Irwindale Gateway Specific Plan; 2) Zone Ordinance Amendment (ZOA) No. 02-2023 to adopt the Irwindale Gateway Specific Plan; 3) Zone Change (ZC) No. 02-2022 to change the Zoning Map designation from M-2 (Heavy Manufacturing) to Irwindale Gateway Specific Plan; and Tentative Parcel Map (TPM) No. 83854 to subdivide the site into seven (7) parcels for property located at 13620 Live Oak Lane – APNs: 8532-002-046 and 8532-002-047; and
- (ii) The proposed Irwindale Gateway Specific Plan (SP), provides for the planned use and long-term development of the property over the next several years; and
- (iii) As a result, the following entitlements are being considered for approval for The Irwindale Gateway Specific Plan:
 - Environmental Impact Report (SCH #2023020290)
 - General Plan Amendment No. 02-2022
 - Zone Ordinance Amendment No. 02-2023
 - Zone Change No. 02-2022
 - Tentative Parcel Map No. 83854
- (iv) In compliance with Public Resources Code Section 21080.4, a Notice of Preparation (“NOP”) was released by the City for public comment on February 10, 2023 and concluded on March 11, 2023. Eleven (11) comments were received during the comment period. The NOP was distributed for public comment to the State Clearinghouse, Office of Planning and Research, responsible agencies, and other interested parties for a 30-day public review. During the 30-day comment period, a Public Scoping meeting was held on March 2, 2023 to gather information for the scope of the Draft EIR. The meeting was held in the Senior Center Dining

Room (16116 Arrow Highway) at 6:00 PM. The oral and written comments received during the meeting were addressed in the Draft EIR; and

- (v) Pursuant to the authority and criteria contained in CEQA, and the City of Irwindale environmental guidelines, the City of Irwindale, as the Lead Agency, has analyzed the project and has prepared an Environmental Impact Report (“EIR”) for the Specific Plan project. The Notice of Availability related to the EIR was released by the City for public comment on May 13, 2024 and concluded on June 27, 2024.
- (vi) Said comments were responded through a Comments and Responses section as part of the FEIR for the Specific Plan; and
- (vii) A copy of the EIR was circulated through the State Clearinghouse (SCH# 2023020290), posted on the City’s website, and was available at the Irwindale Public Library, City Clerk’s Office, and Community Development Department. A copy of the EIR and Mitigation Monitoring and Reporting Program (Exhibit C) was posted on the project’s dedicated website <https://www.irwindaleca.gov/590/13620-Live-Oak-Lane---Irwindale-Gateway->; and
- (viii) The public review period for the Draft EIR ended on June 27, 2024, and ten (10) comments were received during the comment period; and
- (ix) A Final Environmental Impact Report (“FEIR”), SCH# 2023020290 was prepared for the proposed project, including the Draft EIR, comments received on the Draft EIR and responses to those comments, and revisions and corrections to the Draft EIR made in response to comments received, and CEQA Findings of Fact (FOF) and a Statement of Overriding Considerations (SOC) (Exhibit B); and
- (x) The FEIR for the proposed Specific Plan provides an assessment of the environmental impacts, alternatives, and mitigation measures associated with the Specific Plan, and has been prepared in accordance with Public Resources Code Section 21000 *et seq.* (CEQA), and State regulations in Title 14 of the California Code of Regulations, Section 15000 *et seq.* (CEQA Guidelines); and
- (xi) The Planning Commission of the City of Irwindale (Planning Commission) has considered the FEIR and its implementing actions (State Clearinghouse No. SCH# 2023020290) for the Specific Plan, which provides, among other provisions, for the planned use and long-term development of the industrial/commercial business park; and

- (xii) Notice of public hearing before the Planning Commission concerning its consideration of the FEIR was given in accordance with the laws, including CEQA, and policies of the City of Irwindale; and
- (xiii) On October 28, 2024, the Irwindale Planning Commission continued the hearing on this item to a date certain (November 26, 2024); and
- (xiv) The Special Planning Commission Meeting of November 26, 2024 was later rescheduled to November 14, 2024, and
- (xv) Revised notices of public hearing were published, posted, and mailed in accordance with the laws, including CEQA, Government Code and policies of the City of Irwindale; and
- (xvi) On November 14 ,2024, the Planning Commission conducted a duly noticed public hearing on the Irwindale Gateway Specific Plan and corresponding General Plan Amendment No. 02-2022, Zone Change No. 02-2022, Zone Ordinance Amendment No. 02-2023, and Tentative Parcel Map No. 83854 , and the FEIR for the Specific Plan (SCH# 2023020290) at which time, the Planning Commission opened the public hearing, took testimony on the Application, at which time they received input from staff, the Assistant City Attorney, and the Applicant, heard public testimony, discussed the Proposed Project; and closed the public hearing; and
- (xvii) On December 16, 2024, notice of a public hearing before the City Council on the proposed project adoption, was given in accordance with applicable law; and
- (xviii) On January 8, 2025, the City Council continued the hearing on this item to a date certain (January 22, 2025); and
- (xix) On January 22, 2025, the City Council conducted a duly noticed public hearing, as required by law, on the proposed Application at which time oral and documentary evidence was introduced along with the written recommendation of the City of Irwindale City Council, received public testimony, and directed Staff to revise the draft Specific Plan document, Resolutions and Ordinances to remove all references to Battery Energy Storage Systems (BESS), and put on the February 26, 2025 Consent Calendar; and
- (xx) On February 26, 2025, the City Council adopted the revised Resolutions, Ordinances and Specific Plan document; and
- (xxi) All legal prerequisites to the adoption of this Resolution have occurred.

B. RESOLUTION.

NOW, THEREFORE, BE IT RESOLVED THAT THE CITY COUNCIL of the City of Irwindale, having reviewed and considered the information in the certified FEIR and supporting documents and materials, does hereby find, determine, resolve and order as follows:

1. CITY COUNCIL RECORD RECORD

The proceedings and all evidence introduced before the City Council at its public hearing on the Final EIR held on February 26, 2025, 2025, are hereby incorporated into the record of this proceeding. These documents, along with any documents submitted to the decision-makers, including all documents specified under applicable State law shall comprise the entire record of proceedings for any claims under CEQA.

2. FINAL ENVIRONMENTAL IMPACT REPORT CONTENTS

In accordance with CEQA Guidelines, Section 15132, the FEIR consists of the following:

- a. The Draft Environmental Impact Report (DEIR) or a revision of the Draft;
- b. Comments and recommendations received on the DEIR either verbatim or in summary;
- c. A list of persons, organizations, and public agencies comments on the DEIR;
- d. The responses of the Lead Agency to significant environmental points raised in the review and consultation process; and
- e. Any other information added by the Lead Agency.

(All hereafter collectively referred to as "FEIR")

3. ACCOMPANYING DOCUMENTS TO FEIR.

Documents that shall accompany the FEIR are:

- a. Mitigation Monitoring and Reporting Program; and
- b. Findings of Fact; and
- c. Statement of Overriding Considerations

4. CITY COUNCIL REGARDING CEQA FINDINGS OF FACT, MITIGATION MONITORING AND REPORTING PROGRAM AND STATEMENT OF OVERRIDING CONSIDERATIONS

- a. *Adoption of Findings of Fact.* The City Council approves, accepts as its own, incorporates as if set forth in full herein, and make each and every one

of the findings contained in the Findings of Fact, attached as Exhibit “B” of this Resolution.

- b. *Certification of Final Environmental Impact Report.* The City Council hereby certifies that (1) the FEIR has been completed in compliance with CEQA; (2) that it has reviewed and considered the information contained in the FEIR prior to approving the project; and (3) that the FEIR reflects the City Council’s independent judgment and analysis. .
- c. *Adoption of the Mitigation Monitoring and Reporting Program.* As more fully identified and set forth in FEIR and in the Findings of Fact for this Project, which is Exhibit “B” to this Resolution, the City Council finds that the Mitigation Measures described and specifically identified in the above referenced documents are feasible and shall become binding upon the entity (such as the project proponent or the City) assigned thereby to implement the particular Mitigation Measures as identified in the Mitigation Monitoring and Reporting Program.
- d. *Adoption Statement of Overriding Considerations.* Even after the adoption of all feasible mitigation measures and, certain significant or potentially significant environmental effects caused by the Project directly, or cumulatively, will remain. Therefore, the City Council hereby issues and adopts a Statement of Overriding Considerations in the form set forth in the attached Exhibit “B” a copy of which is on file in the office of the City Clerk, identifying the specific economic, legal, social, technological, and other considerations that render the unavoidable significant adverse environmental effects acceptable, either in its current form or as may be modified or amended by the City Council.

BE IT FURTHER RESOLVED THAT a copy of this resolution be transmitted to the City Council and to the Applicant.

The Secretary shall:

- a. Certify to the adoption of this Resolution; and
- b. Forthwith transmit a certified copy of this Resolution, by mail, to the Applicant at the address of record set forth in the Application.

PASSED, APPROVED AND ADOPTED this 26th day of February 2025.

Larry G. Burrola, Mayor

ATTEST:

Resolution No. 2025-01-3589
FEIR, MMRP, SOC
Page 5 of 6

Laura M. Nieto, MMC
Chief Deputy City Clerk
STATE OF CALIFORNIA }
COUNTY OF LOS ANGELES } ss.
CITY OF IRWINDALE }

I, Laura M. Nieto, Chief Deputy City Clerk of the City of Irwindale, do hereby certify that the foregoing Resolution No. 2025-01-3589 was adopted at a regular meeting of the Irwindale City Council held on the 26th day of February 2025, by the following vote of the Council:

AYES: Councilmembers:

NOES: Councilmembers:

ABSENT: Councilmembers:

ABSTAIN: Councilmembers:

Laura M. Nieto, MMC
Chief Deputy City Clerk

Exhibits:

- A. Final Environmental Impact Report (SCH #2023020290)
<https://www.irwindaleca.gov/DocumentCenter/View/9569/Irwindale-Gateway-FEIR-10282024-FINAL-w-appendices>
- B. CEQA Findings of Fact which include: Impacts Determined to be Less Than Significant; Impacts Mitigated to Less Than Significant; Significant Unavoidable Adverse Impacts; Alternatives Considered and Rejected; and Statement of Overriding Considerations
<https://www.irwindaleca.gov/DocumentCenter/View/9604/Irwindale-Gateway-FOF-SOC-FINAL-10142024>
- C. Mitigation Monitoring and Reporting Program
<https://www.irwindaleca.gov/DocumentCenter/View/9605/Irwindale-Gateway-MMRP-FINAL-10242024>

ATTACHMENT "C"

RESOLUTION NO. 2025-02-3590

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IRWINDALE AMENDING THE COMMUNITY DEVELOPMENT ELEMENT AND LAND USE PLAN MAP OF THE CITY'S GENERAL PLAN (GPA NO. 02-2022) FROM REGIONAL COMMERCIAL TO SPECIFIC PLAN FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-047) AS SET FORTH HEREIN AND MAKING FINDINGS IN SUPPORT THEREOF

A. RECITALS.

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- R#GS** All leEFl kææmuisites to t(e Fr oktion o, t(is Resolution (FGæ oppuær .

B. RESOLUTION.

Ny Wf vz dRdTy Rdf it is (eæ6g ,ounr f r eteæ iner Fnr æsolGær 6g t(e hitg hounpil o, t(e hitg o, JÐinr Fle Fs ,olloDsq

1. v(e hitg hounpil (eæ6g skepi,ipFlg ,inr s t(Ft Fl o, t(e ,Fpts set ,oa(in t(e RepitFlsf 9Fæ Af o, t(is Resolution Fæ taue Fnr poææpt.
2. BFser ukon su6stFntiFl eGr enpe kæsender to t(is hitg hounpil r uanE t(e ku6lip (eFânE ponr upter Dit(æEFæ to t(is AkklipFtionf inplur inE Dätten stF,, ækoæðs Fnr GæðFl testic ongf Fnr eGr enpe in t(e hitg's æpoæ ,oa(e kœ@ptf t(is hitg hounpil (eæ6g skepi,ipFlg ,inr s Fs ,olloDsq

F. v(e 9ækeætg DFs kœGouslg pontec kIFter in t(e hitg's 8 eneðFl 9IFn Fs F IFnr ,ill in t(e stFEe o, æplFc Ftion. v(e Nu-WFg biGæ y Fw ReplFc Ftion y keæFtions 9IFn is puææntlg FtptiGæ Fnr e#kepter to 6e poc kleter in Fkkæ#c Ftalg 2027. : kon poc kletion o, æplFc Ftionf t(e kœkeætg Dill 6e EæFr er to c Ftp(t(e Fr Gpent EæFr e. v(e ,inFl æplFic er Fnr æuE(EæFr er sitef ponsistent Dit(t(e kœGous 8 eneðFl 9IFn r esiEnFtionf seæGes Fs t(e 6Fseline ,oa(e JÐinr Fle 8 FteDFg Lkepi,ip 9IFn.

v(e kœkoser p(FnEe in IFnr use r esiEnFtion ,æc ReEionFl hoc c eapiFl to JÐinr Fle 8 FteDFg Lkepi,ip 9IFn is intenr er to enpouæFEe t(e (iE(est Fnr 6est use ,oa(e su6@pt kœkeætg to Euir e t(e r eGelokc ent o, F ,oæ ea IFnr ,ill Fnr sFnr Fnr EæFGel muFæg into F kœr uptiGæ site 6Fs er on its unimue p(FaFpteæstips Fnr Dill 6e in t(e 6est inteææsts Fnr Del,Fæ o, t(e hitg Fnr its æsir ents. v(eæ,oæf t(e kœkoser 8 eneðFl 9IFn Ac enr c ent is ponsistent Dit(Leption hFli,oaniF 8 oGæanc ent hore Leption 7515KFFS Fæ enr c ents§ Z(it r eec s it to 6e in t(e ku6lip inteææstf t(e leEislFtiGæ 6or g c Fg Fc enr Fl oakFæ o, Fn Fr okter EeneðFl kIFn.H

6. v(e kœkoser 8 eneðFl 9IFn Ac enr c ent is inteanFlg ponsistent Dit(Fl ot(ea kœGsions o, t(e 8 eneðFl 9IFn. v(e hoc c unitg CeGelokc ent dlec ent o, t(e 8 eneðFl 9IFn kœc otes seGæðFl kolipies t(Ft sukkoæ t(e Fr oktion o, t(e Lkepi,ip 9IFn Fnr FssopiFter entitlec ents. hoc c unitg CeGelokc ent dlec ent 9olipg l stæGes to kæseaGæ t(e oGæðFl c i# o, IFnr uses Fnr r eGelokc ent in t(e poc c unitg. hoc c unitg CeGelokc ent dlec ent 9olipg 1 seews to ensuæ t(Ft t(e tgkef lopFtionf Fnr intensitg o, Fl neD r eGelokc ent Fnr intensi,ier r eGelokc ents Fr (eæ to t(e æmuiæc ents skepi,ier in its kFaipulFa IFnr use pFteEoæg. hoc c unitg CeGelokc ent dlec ent 9olipg 5 kœc otes t(e poc kœ(ensiGæ r eGelokc ent ponsistent Dit(t(e 8 eneðFl 9IFn Fs okkoser to kiepec eFl Fnr inpæc entFl kIFnninE. J Fr okter f t(e kœkoser 8 eneðFl 9IFn Ac enr c ent Doulr ic klec ent t(e kœkoser JÐinr Fle 8 FteDFg Lkepi,ip 9IFn D(ip(ic klec ents kolipies set

,oat(in t(e 8 eneaFI 9IFnf to kəGr e r iæption ,oat(e lonE-teac r eGelokc ent o, t(e JÐinr Fle 8 FteDFg Lkepi,ip 9IFnf Fr r æsses sustFinF6le kəFptipesf Fnr kəc otes poc kFti6ilitg Dit(suæounr inE FæFs.

p. v(e kəkoser 8 eneaFI 9IFn Ac enr c ent Dill not 6e r etac entFI to t(e ku6lip inteæstf (eFlt(f sF,etgf ponGēnienpef oaDel,Fæ o, t(e hitg. v(e kuəkose o, t(e kəkoser 8 eneaFI 9IFn Ac enr c ent is to ic klec ent t(e Lkepi,ip 9IFnf D(ip(kəGr es F poc kæ(ensiGē ,æFc eDoaw,oat(e lonE-teac r eGelokc ent o, t(e JÐinr Fle 8 FteDFg Lkepi,ip 9IFn Fnr kəGr es plFātg in kəpesses Fnr in t(e pooæ inFtion 6etDeen t(e AkklipFnt Fnr t(e hitg o, JÐinr Fle. J Fr okter f t(e kəkoser Lkepi,ip 9IFn Doulr r e,ine keac itter usesf æEulFte t(e si” es Fnr lopFtions o, 6uilr inEsf skepi,g resiEn Eur elines Fnr r eGelokc ent stFnr Fæ sf Fr r æss c o6ilitg Fnr ponneptiGtg issuesf ir enti,g kə@pt in,æFstauptuæ Fnr seæGpesf Fnr kəGr e Fr c inistæFtiGē Fnr ic klec entFtion IFnEuFEe ,oat(e entiaē 77.7; -Fpæ kəkeatg.

r. v(e F,,epter kəkeaties Fæ k(gspFIlg suitF6le in teac s o, resiEnf lopFtionf okeæFtinE p(FæFpteastipsf s(Fkef si” ef tokoEæFk(gf Fnr t(e kəGSION o, ku6lip Fnr ec eæEnpg Gē(iple Fppessf Fnr ku6lip seæGpes Fnr utilities Fnr is seæGē 6g (iE(DFgs Fnr stæets Fr emuFte in Dir t(Fnr ic kəGēc ent to pFægt(e winr Fnr muFntitg o, tæF,,ip t(e kəkoser use Doulr livelg EeneæFtef to ensuæ t(Ft t(e kəkoser useFSSFnr /oar eGelokc ent Dill not enr FnEeæ @okFæ i” ef oa ot(eæDise ponstitute F (F” Fæ to t(e kəkeatg oa ic kəGēc ents in t(e Gpinitg in D(ip(t(e kəkeatg is lopFter. JÐinr Fle 8 FteDFg Lkepi,ip 9IFn Fnr Fr Gpent enGæns Deæ stur ier ,oa k(gspFIlg suitF6ilitg Fs kFæ o, t(e kækFæFtion ,oat(e Lkepi,ip 9IFn Fnr FssopiFter TinFI dnGænc entFI Jc kFpt Rekoat.

1. 9uæuFnt to t(e Fut(oatg Fnr pateaF pontFiner in t(e hFli,oaniF dnGænc entFI “ uFlitg Apt Rnd” ASo, I 3Mdf Fs Fc enr er f Fnr t(e hitg o, JÐinr Fle enGænc entFI Eur elinesf t(e hitg hounpil ,inr s t(Ft t(e dnGænc entFI Jc kFpt Rekoat RJS,oa v(e JÐinr Fle 8 FteDFg Lkepi,ip 9IFn RltFte hleFânE(ouse Rlh z S No. 2021020230\$ inplur inE t(e – itiEFtion – onitoânE Fnr RekoainE 9æEæFc Fnr Tinr inEs o, TFpt Fnr LtFtec ent o, y Gæar inE h onsir eæFtions Fr emuFtelg r espæ6es t(e 9æ@pt Fnr its kotentiFI ic kFptsf Fs Dell Fs t(e ic kFpts kotentiFIlg æsultinE ,æc t(e FkkæGFI o, t(e JÐinr Fle 8 FteDFg Lkepi,ip 9IFn ,oat(e kuəkoses o, h d” A æGeDf Fnr t(e kəkoser 8 eneaFI 9IFn Ac enr c ent No. 02-2022 is ponsistent Dit(t(e FnFlgsis o, t(e 9æ@pt Dit(in t(e TinFI dnGænc entFI Jc kFpt Rekoatf – itiEFtion – onitoânE Fnr RekoainE 9æEæFc f Tinr inEs o, TFptf Fnr LtFtec ent o, y Gæar inE h onsir eæFtions.

; . BFser ukon t(e su6stFntiFI eGr enpe Fnr ponplusions set ,oat((eæin F6oGē Fnr ukon ot(eaoæFI eGr enpe kæsenter Ft t(e ku6lip (eFânEf Fnr su6@pt to t(e hitg hounpil ponsir eânE Fnr Fr oktinE t(e TdJrf peæi,ier 6g t(e hitg o, JÐinr Fle Fs t(e beFr AEenpgf t(is hitg hounpil (eæ6g æpoc c enr s t(Ft t(e hitg hounpil Fr okt 8 eneaFI 9IFn Ac enr c ent No. 02-2022f D(ip(Fc enr s t(e hoc c unitg

CeGelokc ent dlec ent Fnr bFnr : se 9IFn – Fk FttFp(er d#(i6it ZAH\$,æc ReEionFI hoc c epiFI to JÐinr Fle 8 FteDFg Lkepi,ip 9IFn.

BE IT FURTHER RESOLVED THAT F pokg o, t(is æsolution 6e tæFnc itter to t(e hitg hounpil Fnr to t(e AkklipFnt.

v(e LepætFæg s(Fllq

F. h eði,g to t(e Fr oktion o, t(is ResolutionxFnr

6. Toæ(Dit(tæFnc it F peði,ier pokg o, t(is Resolutionf 6g c Filf to t(e AkklipFnt Ft t(e Fr r æss o, æpoæ set ,oæ(in t(e AkklipFtion.

9ALLdCf A99Ry VdC ANC ACy 9vdC t(is 22^{nr} r Fg o, OFnuFæg 2025.

bFæg 8 . BuæoIFf – Fgoa

AvvdLvq

bFuæF – . Nietof – – h

h (ie, Cekutg hitg h leaw

LvAvd y T h AbJTy RNJA Y

hy : Nv} y T by L AN8 dbdL Yss.

h Jv} y T JRWJNCAbd Y

J bFuæF – . Nietof h (ie, Cekutg hitg h leawo, t(e hitg o, JÐinr Flef r o (eæ6g peði,g t(Ft t(e ,oæEoinE Resolution No. 2025-01 -15K3 DFs Fr okter Ft F æEulFa c eetinE o, t(e JÐinr Fle hitg hounpil (elr on t(e 22^{nr} r Fg o, OFnuFæg 2025f 6g t(e ,olloDinE Gote o, t(e hounpilq

A} dLq hounpilc ec 6ææq

Ny dLq hounpilc ec 6ææq

ABLdNvq hounpilc ec 6ææq

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Resolution No. 2025-02-1530

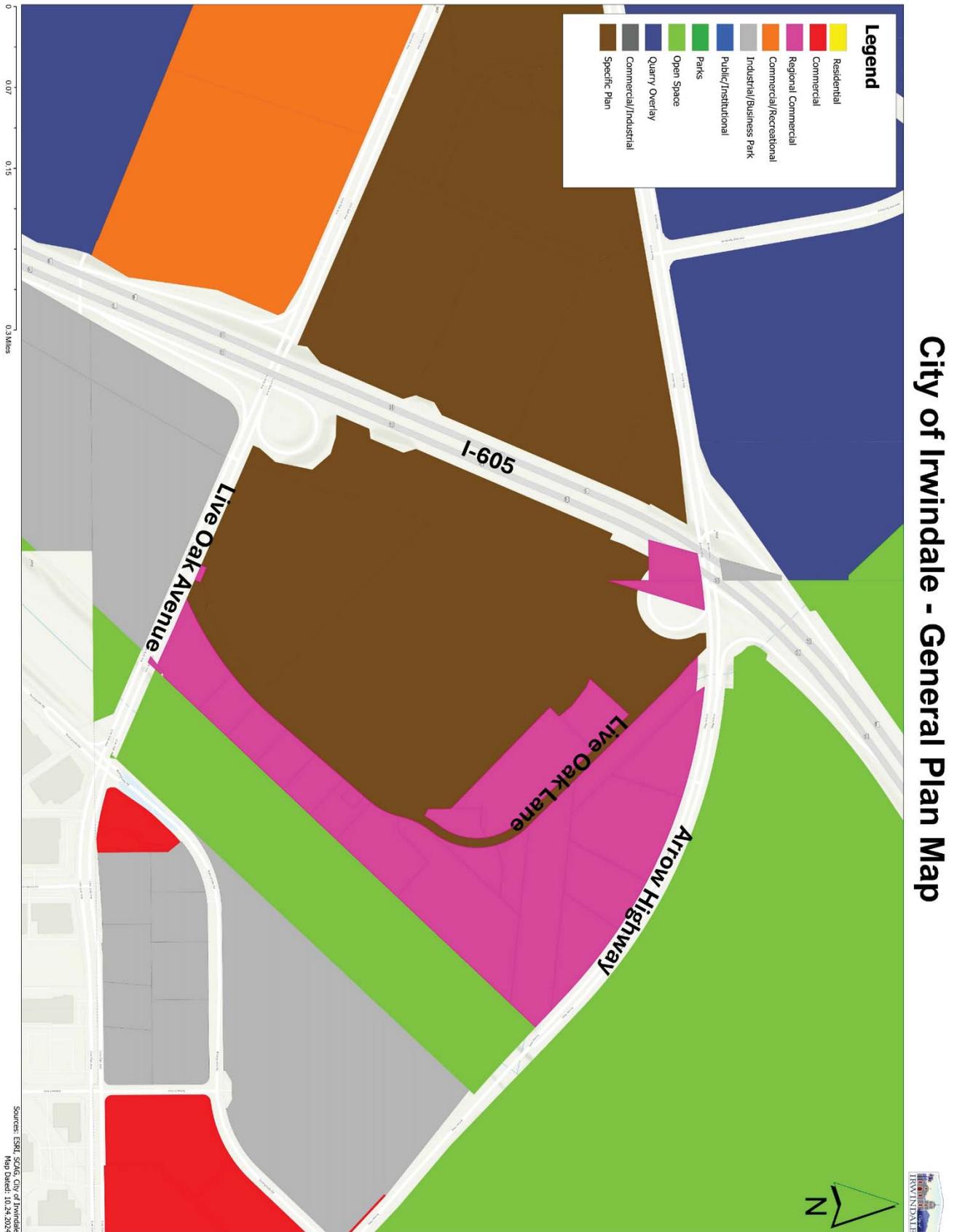
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EXHIBIT "A"



ATTACHMENT "D"

ORDINANCE NO. 787

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF IRWINDALE, CALIFORNIA, ADOPTING ZONE ORDINANCE AMENDMENT NO. 02-2023 TO ADD IRWINDALE GATEWAY SPECIFIC PLAN TO TITLE 17 OF THE IRWINDALE MUNICIPAL CODE (IMC), DEFINING PERMITTED USES, REGULATING THE SIZES AND LOCATIONS OF BUILDINGS, SPECIFYING DESIGN GUIDELINES AND DEVELOPMENT STANDARDS, ADDRESSING MOBILITY AND CONNECTIVITY TOPICS, IDENTIFYING PROJECT INFRASTRUCTURE AND SERVICES, AND PROVIDING ADMINISTRATIVE AND IMPLEMENTATION LANGUAGE FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-047) AND FINDING THE PROJECT CONSISTENT WITH THE CERTIFIED FINAL ENVIRONMENTAL IMPACT REPORT (SCH# 2023020290)

WHEREAS the City Council of the City of Irwindale, California, on July 11, 2023, adopted Ordinance No. 787, which amends the Irwindale Municipal Code (IMC) to add the Irwindale Gateway Specific Plan to Title 17 of the IMC, defining permitted uses, regulating the sizes and locations of buildings, specifying design guidelines and development standards, addressing mobility and connectivity topics, identifying project infrastructure and services, and providing administrative and implementation language for property located at 13620 Live Oak Lane, Irwindale, CA (APNS: 8532-002-046 and 8532-002-047) and finding the project consistent with the Certified Final Environmental Impact Report (SCH# 2023020290);

WHEREAS the City Council of the City of Irwindale, California, on July 11, 2023, adopted Ordinance No. 787, which amends the Irwindale Municipal Code (IMC) to add the Irwindale Gateway Specific Plan to Title 17 of the IMC, defining permitted uses, regulating the sizes and locations of buildings, specifying design guidelines and development standards, addressing mobility and connectivity topics, identifying project infrastructure and services, and providing administrative and implementation language for property located at 13620 Live Oak Lane, Irwindale, CA (APNS: 8532-002-046 and 8532-002-047) and finding the project consistent with the Certified Final Environmental Impact Report (SCH# 2023020290);

WHEREAS the City Council of the City of Irwindale, California, on July 11, 2023, adopted Ordinance No. 787, which amends the Irwindale Municipal Code (IMC) to add the Irwindale Gateway Specific Plan to Title 17 of the IMC, defining permitted uses, regulating the sizes and locations of buildings, specifying design guidelines and development standards, addressing mobility and connectivity topics, identifying project infrastructure and services, and providing administrative and implementation language for property located at 13620 Live Oak Lane, Irwindale, CA (APNS: 8532-002-046 and 8532-002-047) and finding the project consistent with the Certified Final Environmental Impact Report (SCH# 2023020290);

WHEREAS the City Council of the City of Irwindale, California, on July 11, 2023, adopted Ordinance No. 787, which amends the Irwindale Municipal Code (IMC) to add the Irwindale Gateway Specific Plan to Title 17 of the IMC, defining permitted uses, regulating the sizes and locations of buildings, specifying design guidelines and development standards, addressing mobility and connectivity topics, identifying project infrastructure and services, and providing administrative and implementation language for property located at 13620 Live Oak Lane, Irwindale, CA (APNS: 8532-002-046 and 8532-002-047) and finding the project consistent with the Certified Final Environmental Impact Report (SCH# 2023020290);

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Resolultion N. 202
5RAN - 131-18
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é Uö) INeZr suaKleN7 Gua E8M1- 1Z DeNyFf Pyl i 12M1- 1Z. A tNLa N Öi h(v) uK tæyÖÖs DeNyFf Öi wÖÖ DCueÖ FfNyK mDzj 1- 18- 1- 1d- dMLNÖs N Öi DöÖK) igKÖMul s) uK uLuÖgÖ uÖÖi (é ö suÖ 9ygÖ JögeæMDÖ DCÖÖK Rillöi Mul s DN7 7yl Ö Si U ÖL7 il ÖSi LueÖ il ÖA tNLa N Öi h(v ul s GÖFuÖ GN ÖÖF ul s viLNÖF 9eNFu7) uK LNÖs N Öi LeNtÖK sisöuÖs) igKÖ f ÖKÖ)) .é ö suÖtu.FNU4 d- 4E8, 1- 3ÖJ 3Rub3Jul i 33(é ö suÖC3uÖ) ua3Tul s

WHEREAS Öi LygÖ é Uö) Li eNs INeÖi SaulCh(v il sis N Pyl i 12M1- 1ZT ul s

WHEREAS Mu Hö uChl UöN 7 il ÖC(7 Lut Övi LNÖmDzj 1- 18- 1- 1d- c) uK Lé Lué s INeÖi LeLNK s LeNtÖÖtÖsöF Öi SaulCh(v M N7 7 il Ö é ti Ö s N Öi SaulCh(v ul s é KLN K i K ÖÖNÖK tN7 7 il Ö Mul s é UÖN Kul s tNæi tÖN K ÖÖi SaulO h(v 7 usi ö é KLN K i Ö tN7 7 il Ö é ti Ö s Tul s

WHEREAS, N Rt Ögi e10M1- 1ZMÖi (é ö suÖ 9Öll ö F DN7 7 ÖÖN tN Ö yi s ÖÖfi uö FN ÖÖK Ö7 Ö nu suÖ ti eÖö m NU 7 gi e1, M1- 1ZcTul s

WHEREAS Öi wLi tæ ÖÖll ö F DN7 7 ÖÖN 7 ii Ö FN n NU 7 gi e1, M1- 1Z) uK ÖÖeé Kfi syÖs Ö n NU 7 gi eEZM1- 1ZMÖl s

WHEREAS, é UÖi s l NÖi KN LygÖ fi uö F) i é LygÖÖfi s MLNÖs Mul s 7 uÖs ö uttNesul ti) Ö Öi Ö) KÖtÖsöF Dh# AMÖi q NU é 7 il Ö DNöi Mul s LNÖ ö KN Öi DÖÖN (é ö suÖTul s

WHEREAS M n NU 7 gi eEZM1- 1ZMÖi 9Öll ö F DN7 7 ÖÖN tN sytÖs u syÖ l NÖi s wLi tæ LygÖ fi uö FN Öi (é ö suÖ quÖ) uawLi tÖö 9Öl ul s tNæi KLN söF 5N i Resöul ti A7 il s 7 il Ö n N - 131- 18 Mul s usNLÖs 9D vi KNÖÖ n N 08, mZdM é tN7 7 il söF Ö uÖÖi DÖÖ DNyl t Ö usNLÖul Nesöul ti usNLÖF Öi (é ö suÖ quÖ) ua wLi tÖö 9Öl Tul s

WHEREAS, l NÖi N u LygÖ fi uö F gi l Nö Öi DÖÖ DNyl t ÖÖN Öi LeLNK s wLi tÖö 9Öl usNLÖN M) uK FÖl l ö uttNesul ti) Ö uLLÖugÖ Ö) Tul s

WHEREAS, N Pul yuæ 0M1- 1rMÖi (é ö suÖ 9Öll ö F DN7 7 ÖÖN tN Ö yi s ÖÖfi uö FN ÖÖK Ö7 Ö nu suÖ ti eÖö m Pul yuæ 11M1- 1rcTul s

WHEREAS M Pul yuæ 11M1- 1rMÖi DÖÖ DNyl t ÖÖN sytÖs u syÖ l NÖi s LygÖ fi uö FMuK é kyöi s ga Ö) MN Öi LeLNK s ALLÖ uÖN uÖ) fÖf Öi NuCul s sNty7 il Öæ i Us il ti) uK Ö ÖN sytis uÖl F) Ö Öi) eÖl é tN7 7 il suÖN N Öi DÖÖN (é ö suÖ DÖÖ DNyl t ÖÖ ti Ö s LygÖ ÖKÖ N aMul s söi tÖs wÖll Öé UÖ Öi seulÖwLi tÖö 9Öl sNty7 il Ö M i KNÖÖN Kul s Resöul ti K Ö é 7 NU uCä li é l ti K Ö ' uÖÖæ hli eFa wÖNuFi wakÖ7 K mhwwdMul s LyÖN Öi Hi geyuæ 1, M1- 1r DN K i Ö DuÖl sueTul s

WHEREAS MN Hi geyuæ 1, M1-1r MØi Dæ DNyltæCusNLØs Øi æ UK s vi KNØN KVRæð ul ti Kul s wLitæð 9Øl sNy7 il Culs

WHEREAS Mu tNLa N Øi LeNLK s wLitæð 9Øl uKLæ K l Øs N Øi Dæ DNyltæCæN læ) æ Øi DN77yl æ Si U NL7 il OSi LuæØ il MDæ DCæKRllØi M ul s ussæN uCLæ Øs tNLØ Kuæ uæN LNKØs uØØi (æ) Ø suC 9ygæ JæuæMuls ul i CtæN Ø tNLæ KLæUsi s N Øi LeNtæKsi sæuØs) i gLuFi Tul s

WHEREAS, Øi Dæ DNyltæCfuK tN Ksi æ s Øi KØll æ LNeOuls uæN Øi Ø INæ7 uæN Mi Usil ti Muls ØKØ7 N æ t i d s uØØi Lygæ fi uæ FMØt ØsØF Øi æ tN77 il suæN N Øi 9Øl l Ø FN77 æKN Tul s

WHEREAS MuæCæFuCLæ æ kyæKØK N Øi usNLæN N ØK vi KNØN fuU Nt yæi s.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF IRWINDALE DOES ORDAIN AS FOLLOWS:

SECTION 1. :fi vitæKugNU uæ æji ul s tNæ tOuls Ø tNLæNuØs fi æ Ø ga ØKæ li æ l ti .

SECTION 2. ' uK s yLN KygKØl æ Ci Usil ti Læ K l Øs N Øi Dæ DNyltæC syæF Øi Lygæ fi uæ FMØt ØsØF) æØl KØll æ LNeKuls U æguæKØ7 N æMæN) fæf KfuæCtN KæyØ Øi il æi æ tNæ N ØK 7 uæMØi Dæ DNyltæCfi æ ga lØsK ul s si Øæ7 Øi Kp

u. :fi (æ) Ø suC quØ) ua wLitæð 9Øl æ tN KæØl O) æ Øi (æ) Ø suC qili æCØØl MuKu7 il si s ØæNyff qili æCØØl A7 il s7 il OnN - 131-11. :fi 9æNLi ææ) uKLæ UNyKæ tN Ø7 LØØs Ø Øi DææKqili æCØØl uKuØl slæC Øi KØFi N æ t Ø7 uæN. :fi ny3 ua JæU Rub vitæØ7 uæN RLi æN K 9Øl æ tyæi l æ ut æ ul s i BlitØs N gi tN7 LCØs Ø uLLæNæ7 uææ 1-1, . WLN tN7 LCæN N æ t Ø7 uæN MØi LeNLi ææ) ægi Fæusi s N7 uØf Øi us-ut il Cæusi . :fi lØuæ tæ7 is ul s æNyff Fæusi s KæMæN KæØl O) æ Øi Læ UNyKqili æC 9Øl si Kæfi uæN Mæ æJ KuKØi guKæ Øi INæØi (æ) Ø suC quØ) ua wLitæð 9Øl .

qili æCØØl A7 il s7 il OnN - 131-11 LeNti Kæ stN tyæi l æ si Kæfi uæKØi Kæ uKØi (æ) Ø suC quØ) ua wLitæð 9Øl . :fi (æ) Ø suC quØ) ua wLitæð 9Øl æ Ø Øl si s N il tNyæFi Øi fæffi KØul s gi KØyK INæØi Kyg+ tæLæLi ææ N Fysi Øi si U NL7 il ON u INæ7 i eØl slæCul s Kul s ul s FæU Ckyuææ Ø Nu LeNsy t æ Kæ guK s N æKyl ækyi tfæut ææKØ Kul s) ægi Ø Øi gi Kæ Ø æ Kæul s) i Uæ N Øi Dææul s æKæ Ksil æ :fi wLitæð 9Øl æ Øi æ INæ tN KæØl O) æ Øi DææK qili æCØØl Jul s Wæ Si Kæfi uæN Kul s Øi 5N Ø F Si Kæfi uæN MuKu7 il si s.

HyeDi e7 Ne MDi DN77yl a Si U DL7ilOhC7ilONI Di qiliu9Ql LeN7 NOK Ki U euCLN0oK 0uOKyLLNe0Di usNLON Ni Di wLit00 9Ql uls uKNu0s il 007il0 DN77yl a Si U DL7ilOhC7ilO9Na E K0u K0N Le Ki dJ Di NU euC70 Ni 0ls yKi K uls si U DL7ilO0 Di tN77yl a. DN77yl a Si U DL7ilOhC7ilO9Na 8 KibK 0n il Kyé 0u0Di 0Li M0t u0N Mul s 00i K0a Ni u0Ci) si U DL7ilOuls 00i K00s si U DL7il0 usfi é 0 Di é ky07il0 Klit00s 0 0X Lu0y0e 0ls yKi tu0FN0. DN77yl a Si U DL7ilOhC7ilO9Na r LeN7 NOK Di tN7 Léfil K0 si U DL7ilO tN K0i O) 00 Di qiliu9Ql uK NLLNKs 0 L0ti7iuCuls 0té7il0C L0l 0F.:fi (é 0suC qu0) uawLit00 9Ql 0 LC7il0 LN00 KKi 0N00 0 Di qiliu9Ql M0N LeN0si s0it 0N INeDi si U DL7ilONI Di gyK0i K Lu0 NU e Ki U euC ai ueM ussé KKi K KyK00 ugC Lut 0i K N uls NI3K0M LeN7 NOK tN7 Lu000a) 00 Ky0Nyl s0F ue uK Mul s LeN0si Ku yl 0kyi uls IC B0C LN0a sNty7il00u0ut 000KNesi éa si U DL7ilONI Di K0.

g. ' uKis Ni Di ugnU l0s0FKMDi (é 0suC qu0) uawLit00 9Ql uls Di D0a DNylt 0KusNLON Di é N tN7 L0K) 00 qNU é 7il0DNsi wit 0N , rZr- et seq.

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IRWINDALE GATEWAY SPECIFIC PLAN

PREPARED BY
SAGECREST PLANNING+ENVIRONMENTAL



SAGECREST
PLANNING + ENVIRONMENTAL

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INTRODUCTION

I.

1. Introduction

1.1 Overview

The Irwindale Gateway Specific Plan (“Specific Plan”) provides a comprehensive land use program to guide the development of an approximately 66.64-acre property in the City of Irwindale, California. The Specific Plan covers an approximately 52.65-acre developable portion (the “Development Area”) and 13.99 acres of undevelopable area (SCE easement and roadway dedications). The Specific Plan land use concept features an industrial business park with up to 997,796 square feet of building space. The Development Area is envisioned as a place where industrial businesses can thrive and encourage further economic investment in the City of Irwindale (the “City”). Its contemporary design will enhance the visual quality of the currently vacant site.

Notwithstanding anything to the contrary contained herein, any improvements shown on the figures contained herein are conceptual and for illustrative purposes only, and it shall not be a requirement that improvements be located, designed, or constructed as shown on such figures.

1.2 Background

The Development Area is the site of a former quarry. In the late 1980’s, quarry activities ceased, and the site was designated for reclamation as required by the State Surface Mining and Reclamation Act of 1975 (SMARA). On December 15, 1994, the City adopted Resolution 94-55-1381, approving a conditional use permit authorizing the operation of an inert landfill to “fill and restore the depleted gravel pit.” In connection with the approval, the City prepared and certified an Environmental Impact Report, State Clearinghouse No. 88060819, and adopted a related Statement of Overriding Considerations. Additionally, there are three existing billboards on the site. An approved grading plan was issued on September 16, 2022 to fix previous inert material that had been improperly placed. The inert debris remedial grading operation is bringing the site back to productive use with geotechnically certified fill.

Figure 1-1 – Site Photo

1.3 Description of Specific Plan Contents

This Specific Plan guides development of the Development Area by setting forth a land use plan, transportation and circulation plan, infrastructure plans, development standards, and design guidelines that address building placement, architectural style, landscaping, and other design elements. The substantive design elements of the buildings create visual interest, unity, and compatibility through the treatment of exterior building materials, colors, and façades as well as building placement, massing, and scale. In addition, a cohesive landscaping program will provide pleasing views of the Development Area from on- and off-site locations. Altogether, the various elements of the built environment in the Development Area will enhance the visual quality of the site and create an attractive development within the City.

From a long-term operational perspective, the Specific Plan accommodates industrial, and business park. These types of uses serve to encourage economic investment, jobs, and business opportunities in the City of Irwindale and assist in maintaining sustained economic stability and growth.

1.4 Description of Specific Plan Area

As shown on Figure 1-2, Vicinity Map, the Specific Plan area is located in the western portion of the

City of Irwindale, immediately east of I-605, north of Live Oak Avenue and south of Arrow Highway. The City of Baldwin Park is located to the southeast. The I-605 Freeway forms the western boundary of the Specific Plan Area with the I-605/Live Oak Avenue interchange located immediately southwest of the Specific Plan Area and the I-605/Arrow Highway interchange located immediately northwest of the Specific Plan Area.

As shown on Figure 1-3, Aerial Photograph, surrounding land uses include the Southern California Edison Rio Hondo Sub-station, located on the south side of Live Oak Avenue, and the Santa Fe Dam Flood Control Basin, located on the north side of Arrow Highway. The land located east of the Specific Plan Area is used for industrial purposes. Access to the Specific Plan Area is provided through an established local roadway network. The Development Area is within walking distance (half mile) of the nearest Foothill Transit bus stop (Line 492) on Live Oak Avenue and Stewart Avenue.

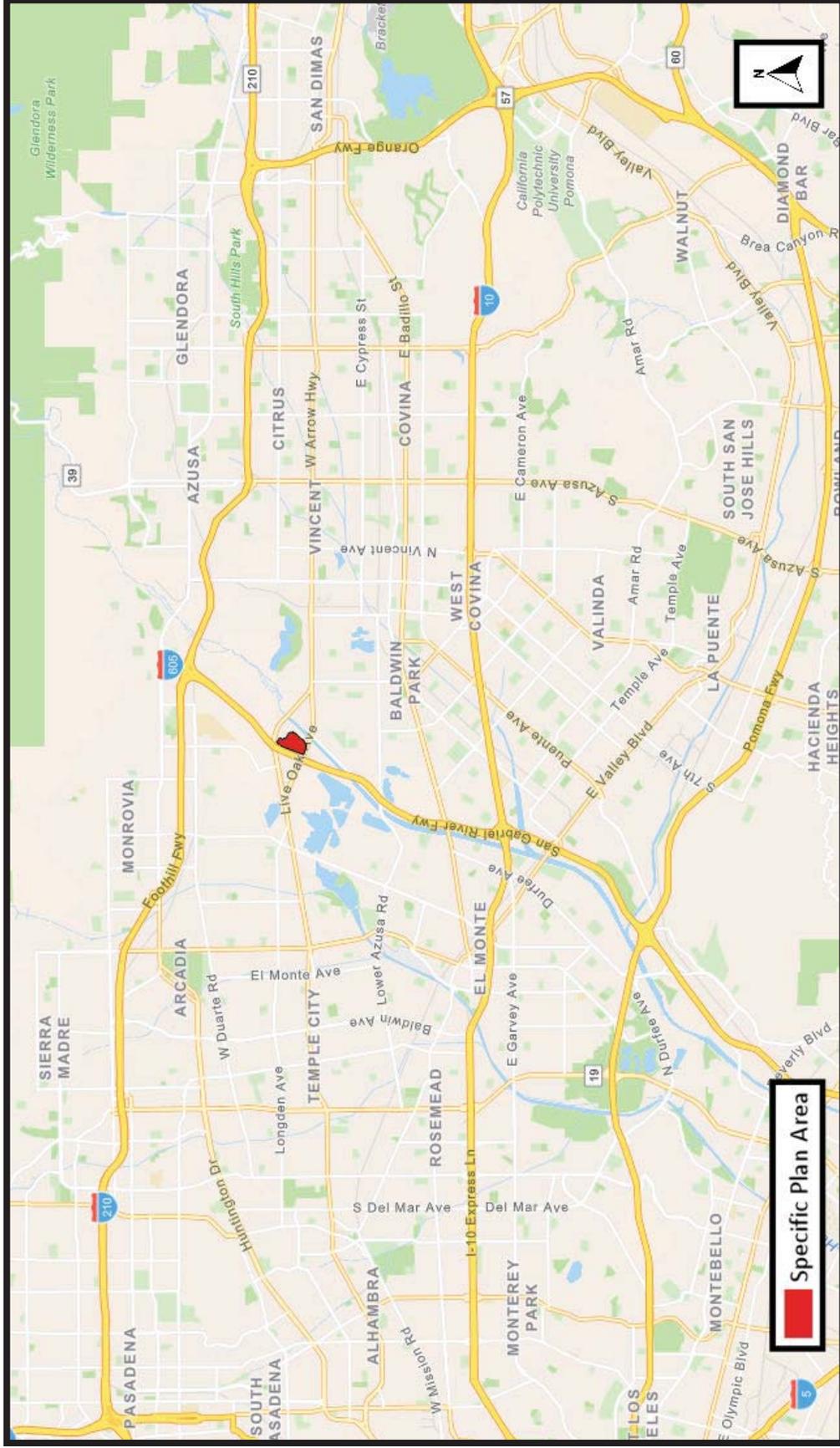


FIGURE 1-2 – VICINITY MAP

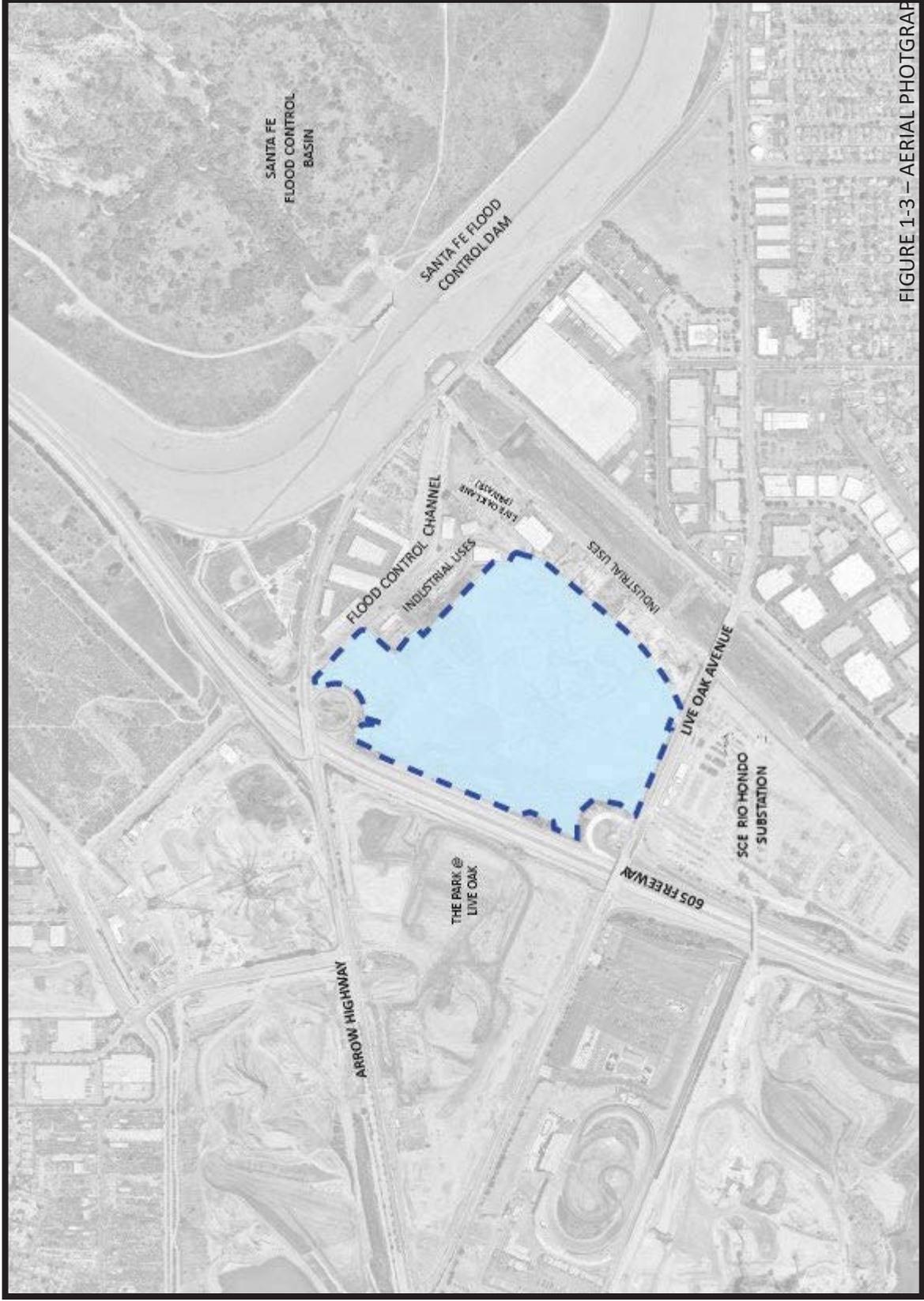


FIGURE 1-3 – AERIAL PHOTOGRAPH

1.5 Specific Plan Organization

This Specific Plan is organized into the following sections:

- Section 1 – Introduction
- Section 2 – Purpose and Intent of Specific Plan
- Section 3 – Relationship to Other Land Use Regulations
- Section 4 – Land Use
- Section 5 – Transportation and Circulation
- Section 6 – Development Standards
- Section 7 – Design Guidelines
- Section 8 – Utility Infrastructure Plan
- Section 9 – Implementation

Unless otherwise defined in this Specific Plan, the meaning of capitalized words, phrases, titles, and terms shall be the same as provided in the City of Irwindale Municipal Code and the City of Irwindale Commercial and Industrial Design Guidelines.

Purpose and Intent of Specific Plan

II.

2. Purpose and Intent of Specific Plan

2.1 Purpose and Intent

This Specific Plan, adopted by ordinance, establishes the development regulations, allowable land uses, design guidelines, and implementation procedures for the Development Area. The Specific Plan establishes the effective zoning for the Development Area. In addition, circulation and other infrastructure improvements such as water, wastewater, and stormwater drainage systems are addressed by this Specific Plan to ensure their proper sizing and timely installation.

2.2 Specific Plan Authority

This Specific Plan is a regulatory document prepared pursuant to the provisions of California Government Code §§ 65450 through 65457, which grant local government agencies the authority to prepare specific plans for the systematic implementation of their general plan for all or part of the area covered by the general plan.

California Government Code §§ 65450 through 65457 establish the authority to adopt a specific plan, identify the required contents of a specific plan, and mandate consistency with the general plan.

According to California Government Code § 65451:

- (a) A specific plan shall include text and a diagram or diagrams which specify all of the following in detail:
 - (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
 - (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
 - (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
 - (4) A program of implementation measures including regulations, programs, public works projects, and financing measures, necessary to carry out items (1), (2), and (3)
- (b) The specific plan shall include a statement of the relationship of the specific plan to the general plan.

This Specific Plan includes each of the required elements listed above and establishes the essential link for how the Specific Plan is consistent with the policies of the City of Irwindale General Plan (the "General Plan"). The Specific Plan provides the City of Irwindale's staff and decision makers, as well as developers, investors, the community at-large, utility providers, public service districts, and others with a comprehensive plan establishing regulations, conditions, and programs for guiding the systematic development of the Development Area. All future development plans and implementing

actions within the Development Area are required to be consistent with the regulations set forth in this Specific Plan and with all other applicable City, County, State, and federal regulations.

2.3 Statement of Objectives

To ensure the functional integrity, economic viability, and positive aesthetic appearance of the Development Area, the following planning and development goals are established:

- Create a comprehensive land use plan for the re-use of a reclaimed sand and gravel quarry.
- Provide guidelines and standards for the development of state-of-the-art buildings that can accommodate various industrial and manufacturing uses, including warehouse distribution, logistics, and fulfillment centers with proximate access to Interstate 605 on- and off-ramps.
- Ensure that infrastructure plans for water, sewer, drainage, dry utility, and solid waste disposal are adequately designed for the Specific Plan.
- Provide a circulation system that meets transportation requirements and minimizes potential adverse impacts on the surrounding area.
- Provide guidelines and standards for architecture, landscaping, walls, fencing, lighting, and entry treatments that are compatible with the design and architecture of the surrounding uses.

Relationship to other Land Use Regulations

III.

3. Relationship to Other Land Use Regulations

California Government Code § 65454 requires a specific plan to be consistent with the local General Plan. As part of the adoption of this Specific Plan, a General Plan Amendment changes the General Plan land use designation for the Specific Plan Area from “Regional Commercial” to “Specific Plan”, and a Zoning Ordinance Amendment changes the zoning designation for the Specific Plan Area from “M2 (Heavy Manufacturing)” to “Irwindale Gateway Specific Plan”. The adoption of this Specific Plan establishes the zoning regulations for the Specific Plan Area in order to implement the General Plan land use designation of “Specific Plan” for the Specific Plan Area.

The Specific Plan provides for a land use plan and development standards, which reflect the objectives in Section 2, governing development of the principal physical components of the Development Area, including, but not limited to: buildings, circulation and parking facilities, landscaping, open space, and utility improvements. The Specific Plan provides for the standards that establish the general type, parameters, and character of development with the goal of creating an integrated and unified environment that is compatible with its surrounding area. The Specific Plan also provides a plan for infrastructure improvements to serve the Development Area including the development of a new detention basin within the SCE Easement Area for collection of project storm water flows. The detention basin will replace an existing, unimproved natural retention basin located within the SCE Easement Area.

3.1 General Plan Consistency

The Specific Plan is designed to meet the goals established in the City’s General Plan by providing a framework for the future development of the Development Area. The Specific Plan is designed to be consistent with, and serves as an extension of, the City’s General Plan. The Specific Plan is consistent with the following General Plan Land Use Element policies and objectives:

GENERAL PLAN POLICY	SPECIFIC PLAN CONSISTENCY
COMMUNITY DEVELOPMENT ELEMENT (CDE)	
<i>Issue Area – Land Use Planning: The City of Irwindale is committed to the development of a comprehensive land use plan that will enhance the City’s livability and economic base for future generations.</i>	
CDE Policy 1: The City of Irwindale, through continued comprehensive land use planning, will strive to preserve the overall mix of land uses and development in the community.	The Specific Plan is consistent with the City’s goal of preserving the overall mix of land uses and development in the community by setting forth a land use plan and development regulations for how the reclaimed former quarry located in the Development Area will accommodate industrial and business park uses.

CDE Policy 2: The City of Irwindale will continue to plan for the transition of the quarries located within the City to other land uses.	The Development Area is a remediated former quarry. The Specific Plan allows the transition of the site to a productive and economically beneficial development for the City of Irwindale.
CDE Policy 3: The City of Irwindale will continue to ensure that the type, location, and intensity of all new development and intensified developments adhere to the requirements that are specified for their particular land use category in the General Plan.	The Specific Plan provides a development plan, infrastructure plans, development standards, and design guidelines that address building placement, architectural style, landscaping, and other design elements to address the specific characteristics of the site consistent with the General Plan land use designation of Specific Plan.
CDE Policy 5: The City of Irwindale will continue to promote comprehensive development consistent with the General Plan as opposed to piecemeal and incremental planning.	The Specific Plan implements policies set forth in the General Plan by providing a comprehensive plan and direction for the long-term development of the Development Area. The Specific Plan provides for implementation of uniform development regulations and design guidelines and addresses sustainable practices, to promote compatibility with surrounding areas. Adoption of the Specific Plan provides for the comprehensive development of the Development Area, thereby avoiding planning of piecemeal development.
<i>Issue Area – Economic Development: The City of Irwindale intends to continue its pursuit and promotion of economic development that will provide jobs and revenue for the community.</i>	
CDE Policy 10: The City of Irwindale will promote development that will benefit the community as a whole in terms of both jobs and revenue generation.	The Specific Plan allows for up to 997,796 square feet of industrial and business park uses on a former quarry site. Revenue benefits to the City of Irwindale may include but not be limited to increased property tax revenue and point-of-sale tax revenue. In addition, the Specific Plan will allow for development of uses leading to creation of jobs that can be filled by residents of the City and surrounding communities.
<i>Issue Area – Urban Design: The City of Irwindale will continue its efforts in improving the appearance of the community.</i>	
CDE Policy 12: The City of Irwindale will continue to promote quality design in the review and approval of commercial and industrial development through the application of the commercial and industrial design guidelines.	The Specific Plan promotes quality design by providing guidelines and standards for landscaping, architecture, walls, fencing, signage, lighting, and entry treatments addressing the specific character of the site and proposed uses to maintain compatibility with

	the design and architecture of the surrounding uses.
CDE Policy 14: The City of Irwindale will continue to promote property maintenance in all areas of the City.	The Specific Plan defines the entities responsible for maintenance of publicly and privately-owned improvements within the Specific Plan Area, including roadways and utility infrastructure (refer to Section 9.10 and Table 9-1). Compliance with the Specific Plan’s maintenance program ensures that all improvements within the Specific Plan Area are properly and perpetually maintained.
CDE Policy 16: The City of Irwindale will continue to work towards the development of streetscape, sign standards, and a Public Art Program.	The Design Guidelines for the Specific Plan (refer to Section 7) establish comprehensive streetscape design standards for the Irwindale Gateway Specific Plan’s frontage on Arrow Highway, Live Oak Avenue and Live Oak Lane promoting a welcoming visual environment for employees, visitors, and passersby. Signage will conform to City standards.
INFRASTRUCTURE ELEMENT (IE)	
<i>Issue Area – Maintenance of Service Standards: City of Irwindale will continue to maintain the highest levels of public service to respond to the existing and future demand for such services.</i>	
<p>IE Policy 1: The City will continue to support the efforts of the City of Irwindale Public Works Department in maintaining the highest service standards feasible.</p> <p>IE Policy 2: The City will continue to cooperate with those utility providers in the City to ensure that sufficient infrastructure capacity is available to meet current and future service demands.</p>	The Specific Plan requires improvements to be made to roadways and public utilities/infrastructure in conjunction with future development of the Development Area and as required by the City of Irwindale and applicable public service providers. Improvements are required to be made by the developers as necessary to serve the Development Area while maintaining adequate service levels for existing surrounding land uses. Plans for water, sewer, storm water, dry utilities, and solid waste disposal are provided in Section 8, Utility Infrastructure.
<i>Issue Area – Traffic and Circulation: The City of Irwindale will strive to improve safe and efficient circulation in the City.</i>	
<p>IE Policy 3: The City of Irwindale will continue to develop and enhance the existing streets and intersections in the City.</p> <p>IE Policy 4: The City of Irwindale will strive to ensure that all new development implements its “fair-share” of infrastructure</p>	The Specific Plan includes roadway and sidewalk improvement plans to facilitate efficient vehicular and non-vehicular transportation through and around the Development Area. With implementation of the circulation improvements identified in Section 5 of the Specific Plan, traffic impacts

<p>improvements to offset the potential adverse impacts associated with the additional traffic that will be generated by the new development.</p>	<p>generated by development in the Development Area will be offset to the City of Irwindale’s satisfaction. Furthermore, developers of land within the Development Area will be responsible for funding and/or construction of the required infrastructure improvements as set forth by the Specific Plan.</p>
<p>RESOURCE MANAGEMENT ELEMENT (RME)</p>	
<p><i>Issue Area – Natural Resources. The City of Irwindale will continue to cooperate in the maintenance and conservation of the area’s natural resources.</i></p>	
<p>RME Policy 1: The City of Irwindale will continue to work with the quarries and other regulatory agencies to facilitate their reclamation.</p> <p>RME Policy 3: The City of Irwindale will work with the quarry owners and/or operators and regulatory agencies to help facilitate their timely reclamation.</p>	<p>The Development Area is a former quarry site, which is the subject of an ongoing inert debris remedial grading operation being conducted to return the site to productive use. This Specific Plan provides guidelines and development standards for the redevelopment of the site. As such, the Specific Plan provides a comprehensive plan for the transition of the site to a productive and economically beneficial development for the City of Irwindale.</p>
<p><i>Issue Area – Resource Preservation. The City of Irwindale will maintain and preserve those natural and man-made amenities that contribute to the City’s livability.</i></p>	
<p>RME Policy 11: The City of Irwindale supports the ethic of conservation of non-renewable resources. This includes efforts to reduce the use of energy (in any form), greenhouse gas (GHG) emissions (consistent with AB 32) and efforts to find new and more energy efficient methods for delivering services. The City supports the development of building standards that enable the community to design energy saving features such as solar energy systems, water efficient landscaping, and sustainable, green, and energy efficient building standards.</p>	<p>The Specific Plan allows industrial land uses adjacent to Interstate 605 and approximately 1.8 miles from Interstate 210. By its location near major transportation corridors, the Specific Plan has the potential to reduce vehicle miles traveled, which would reduce tailpipe emissions – a major source of greenhouse gases. In addition, buildings in the Development Area will be required to comply with the California Green Building Standards Code, the City’s Model Water Efficient Landscape Ordinance (MWELO), and will incorporate additional sustainable design features that minimize water use and maximize energy efficiency.</p>
<p><i>Issue Area – Mining and Reclamation. The following policies focus on those City policy actions that can be taken to improve environmental compliance, reclamation planning, and long-term economic improvement of the mines and quarries (inactive, active, and reclaimed) in Irwindale.</i></p>	
<p>RME Policy 19: The City of Irwindale will consider environmental justice issues as they are related to potential health impacts</p>	<p>The Development Area is a former quarry that was filled improperly with inert debris and reclaimed beginning in 2023. Air pollution</p>

<p>associated with air pollution and ensure that all land use decisions, including enforcement actions, are made in an equitable fashion to protect residents, regardless of age, culture, ethnicity, gender, race, socioeconomic status, or geographic location from the health effects of air pollution.</p>	<p>emissions historically associated with mining and filling have ceased and air pollutant emissions associated with development of the Specific Plan would be generated primarily by mobile sources (tailpipe emissions from vehicles traveling to and from the property). These air pollutant emissions are regulated at the federal and State levels through mandated vehicle engine performance standards and fuel content requirements. The Development Area is surrounded by flood control uses to the north and east, the Southern California Edison Rio Hondo sub-station to the south, and Interstate 605 and quarry uses to the west. As such, there are no known or probable environmental justice issues associated with developing the Specific Plan land uses.</p>
<p>RME Policy 25: The City of Irwindale will monitor traffic and congestion to determine when and where the City needs new transportation facilities to achieve increased mobility efficiency.</p>	<p>The Specific Plan requires roadway and sidewalk improvements for new developments to facilitate efficient vehicular and non-vehicular transportation through and around the Development Area. The Specific Plan provides for required roadway improvements, including to the north side of Live Oak Avenue and Live Oak Lane. Transportation and roadway improvements identified in Section 5 of this Specific Plan ensure that the segments of Live Oak Avenue and Live Oak Lane adjacent to the Development Area will operate at an acceptable service level.</p>
<p>PUBLIC SAFETY ELEMENT (PSE)</p>	
<p><i>Issue Area – Emergency Preparedness. The City of Irwindale will strive to maintain the highest levels of readiness to respond to disasters or local emergencies.</i></p>	
<p>PSE Policy 3: The City of Irwindale will work to reduce potential hazards through conscientious land use planning. The City shall require liquefaction assessment studies as part of development proposals in areas identified by the California Geological Survey as susceptible to liquefaction. The studies shall be conducted in accordance with the California Geological Survey’s Special Publication 117; Guidelines for Evaluating and Mitigating Seismic Hazards in California, and</p>	<p>The Development Area is the site of a reclaimed former quarry. The inert debris deposited at the property during the reclamation process consists of a combination of crushed rock, fine silt, and clean construction and demolition waste (e.g., broken concrete, asphalt, brick, soil) that complies with applicable State of California, Los Angeles County, and City of Irwindale standards. Prior to backfill, the inert debris was blended, moisture conditioned, and compacted for stability in accordance with State of</p>

<p>the Southern California Earthquake Centers (1999) procedures to implement Special Publication 117 – Liquefaction Hazards (both documents are incorporated herein by reference). On sites shown to be susceptible to liquefaction, the City shall require the implementation of mitigation measures designed to reduce this hazard to an acceptable level. The City shall require a State certified engineering geologist or registered civil engineer; having competence in the field of seismic hazard evaluation and mitigation, to review the study at the Applicant’s expense. The review shall determine the adequacy of the hazard evaluation and proposed mitigation measures and determine whether the requirements of State law are satisfied, as described in Special Publication 117 by the California Geological Survey.</p>	<p>California and City of Irwindale requirements of graded fill materials. Following completion of reclamation, the Development Area will be suitable for development and will not be subject to liquefaction or other geologic stability hazards. Additionally, site and development geotechnical investigations will be required for all subsequent development proposals within the Development Area. City approval of these investigations and incorporation of investigation requirements and recommendations will be required prior to the issuance of grading permits to ensure potential hazards due to liquefaction are addressed.</p>
<p><i>Issue Area – Noise: The City of Irwindale will work to reduce the high levels of noise exposure associated with the existing development and transportation facilities in the City.</i></p>	
<p>PSE Policy 5: The City of Irwindale will work towards reducing noise exposure in the City by considering noise and land use compatibility in land use planning.</p>	<p>The Development Area does not abut noise-sensitive land uses (e.g., residential, schools, etc.). The uses allowed within the Development Area are compatible with the surrounding industrial uses. Furthermore, the potential noise generated by future developments has been analyzed in the Irwindale Gateway Specific Plan EIR (as hereinafter defined) in accordance with the California Environmental Quality Act and potential impacts will be mitigated.</p>

3.2 Relationship to Zoning Code

The Title 17 (Zoning Code) of the Irwindale Municipal Code provides a citywide framework of regulations that addresses topics such as permitted uses, development standards, parking and landscaping regulations, permit procedures, and sign regulations.

The Specific Plan serves as the zoning for the Specific Plan Area, and as such, includes the development regulations applicable to the Specific Plan Area. In the event that any provision in the Specific Plan conflicts with the Zoning code, the provisions set forth within the Specific Plan shall prevail. In the event the Specific Plan is silent regarding any Zoning provision applicable to the Development Area, the Zoning Code shall prevail.

3.3 Subdivisions

All development in the Development Area shall comply with Title 16 (Subdivisions) of the Irwindale Municipal Code and the Subdivision Map Act (Cal. Government Code § 66410, et. seq), as applicable.

Land Use

IV.

4. Land Use

4.1 General Provisions

This section establishes allowable uses within the Development Area. The Specific Plan allows for a variety of uses, including professional offices, and industrial uses.

The Development Area is planned for the development of warehousing, distribution, fulfillment center, industrial, manufacturing, and office uses. The SCE Easement Area is planned for infrastructure improvements required to serve the Development Area. A breakdown of the size of the areas within the Specific Plan is as follows:

Table 4-1

Area	Size
Gross Site Area	66.64 acres
SCE Easement Area	9.61 acres
Right-of-Way Dedication	4.38 acres
Net Developable Area	52.65 acres

4.2 Summary of Land Use Plan

The Development Area may be developed using one of the two following land use plans:

Land Use Plan 1: Consisting of the entire 52.65-acre Development Area (Figure 4-1). If the Development Area is developed to Land Use Plan 1 the entire site would be developed with industrial, manufacturing, warehousing, distribution, fulfillment center, and office uses (collectively, “Industrial and Business Park Uses”). Sites which are developed with industrial, manufacturing, warehousing, distribution, fulfillment center, and/or office uses shall be referred to as “Industrial and Business Park Sites.”

The development of the Development Area according to one of the land uses plans shall not restrict redevelopment of the Development Area according to the other land use plan.

Transportation and Circulation

V.

5. Transportation and Circulation

5.1 Purpose and Intent

The Circulation Plan for the Specific Plan provides a roadway network to meet the vehicular and non-vehicular needs of employees and visitors and for the transportation of goods to and from the Development Area.

5.2 Vehicular Circulation

The Circulation Plan, as illustrated by Figures 5-1 and 5-2, describes the proposed roadway improvements to accommodate traffic generated by the anticipated Land Use Plan. Vehicular access to the Development Area is provided via Live Oak Avenue, adjacent to the southern boundary of the Specific Plan, and Arrow Highway, which abuts a portion of the northern boundary of the Specific Plan, and Live Oak Lane, a private street extending from Live Oak Avenue to Arrow Highway. Figures 5-1 and 5-2 demonstrate access and on-site circulation for conceptual improvements within the Development Area. Figures 5-3 and 5-4 illustrate the typical design of circulation improvements planned as part of the development project.

The following facilities and improvements are proposed for the primary components of the Specific Plan's vehicular circulation network:

Live Oak Avenue

Live Oak Avenue forms the southern boundary of the Specific Plan. This public roadway provides east/west access to the Development Area and a connection point to and from I-605. In its fully improved condition, the segment of Live Oak Avenue abutting the Specific Plan features a 106-foot-wide right-of-way ("ROW"), including 34 feet of pavement in the east travel lane (two lanes), 34 feet of pavement in the west travel lane (two lanes), a 16-foot-wide raised center median that reduces to 5-feet to accommodate an 11-foot left turn lane when needed, and a 9-foot-wide sidewalk with 3-feet of landscape terrace on the south side of the street. As part of the Specific Plan Area's development, a 5-foot-wide meandering sidewalk and a 5-foot meandering landscaped parkway is provided along the north side of Live Oak Avenue. The existing travel lanes will remain. Live Oak Avenue is a designated truck route. On-street parking is generally prohibited on both sides of the roadway. All frontage improvements to Live Oak Avenue are required to comply with applicable City of Irwindale requirements, including sight distance requirements, except as may otherwise be provided herein. Live Oak Lane (described below) connects to Live Oak Avenue. One direct driveway will connect to Live Oak Avenue at the signalized intersection with Graham Road, which will also allow temporary truck access during construction. The Live Oak Avenue and Live Oak Lane intersection shall become signalized to alleviate the additional traffic delay generated by the new developments.

Arrow Highway

Arrow Highway abuts the portion of the northern boundary of the Specific Plan where Live Oak Lane intersects with Arrow Highway. This public roadway provides east/west access to the Development Area and to/from I-605. The segment of Arrow Highway abutting the Development Area is improved

with a 100-foot-wide ROW, including 36 feet of pavement in each direction (2 lanes), a 12-foot-wide raised center median, an existing 5-foot-wide sidewalk 3-foot wide landscaped parkway on both sides of the ROW. The Development Area will not have direct vehicular access to Arrow Highway. Instead, two driveways will provide access to Live Oak Lane on the north side of the Development Area, which provides access to Arrow Highway at an unsignalized right-in-right out intersection. Arrow Highway is a designated truck route. Any frontage improvements to Arrow Highway are required to comply with applicable City of Irwindale requirements, including sight distance requirements, except as may otherwise be provided herein.

Live Oak Lane

Live Oak Lane is an existing collector road that connects the development area to either Live Oak Avenue or Arrow Highway. It is a two-lane undivided road generally oriented in a north-south direction. Live Oak Lane is accessible from public streets and provides the primary ingress and egress for all development within the Development Area. Live Oak Lane will have a 60-foot-wide ROW with 40 feet of pavement and 10 feet of sidewalk with parkway on either side of the street. The pavement area includes a 12' travel lane and an 8' wide parking lane on each side of the street. The Live Oak Avenue and Live Oak Lane intersection shall become signalized to alleviate the additional traffic delay generated by the new developments.

Public Alley

A new public alley is provided to connect the northern and southern sections of Live Oak Lane. The public alley is oriented in the north-south direction. The public alley is an undivided two-lane ROW that has varied width, which ranges from 38-feet to 47-feet. One 13-foot-wide travel lane is provided on each direction and an 8-foot parking lane is provided adjacent to the southbound travel lane. On the east side of the public alley is a grass terrace that ranges from 4-feet to 13-feet wide.

Private Driveways and Drive Aisles

Interior private driveways and drive aisles are proposed to connect individual building sites within the Development Area and provide vehicular access to Live Oak Avenue and Live Oak Lane. Private driveways and drive aisles provide vehicular access for automobiles and trucks to parking lots, truck courts, loading dock areas, etc. The locations, alignments, and widths of private driveways and drive aisles will be determined at the time buildings are designed and positioned as part of implementing development projects and are subject to approval of the Director of Engineering.

Interstate 605

I-605 is located immediately west of the Specific Plan and is under the authority of the California Department of Transportation ("Caltrans"). Because the freeway falls completely outside of the Specific Plan Area's boundary, development of the Development Area does not affect Caltrans' plans for the operation and maintenance of I-605. Drivers of I-605 have convenient access to the Development Area via the on-/off-ramps at Live Oak Avenue and on-/off-ramp at Arrow Highway.

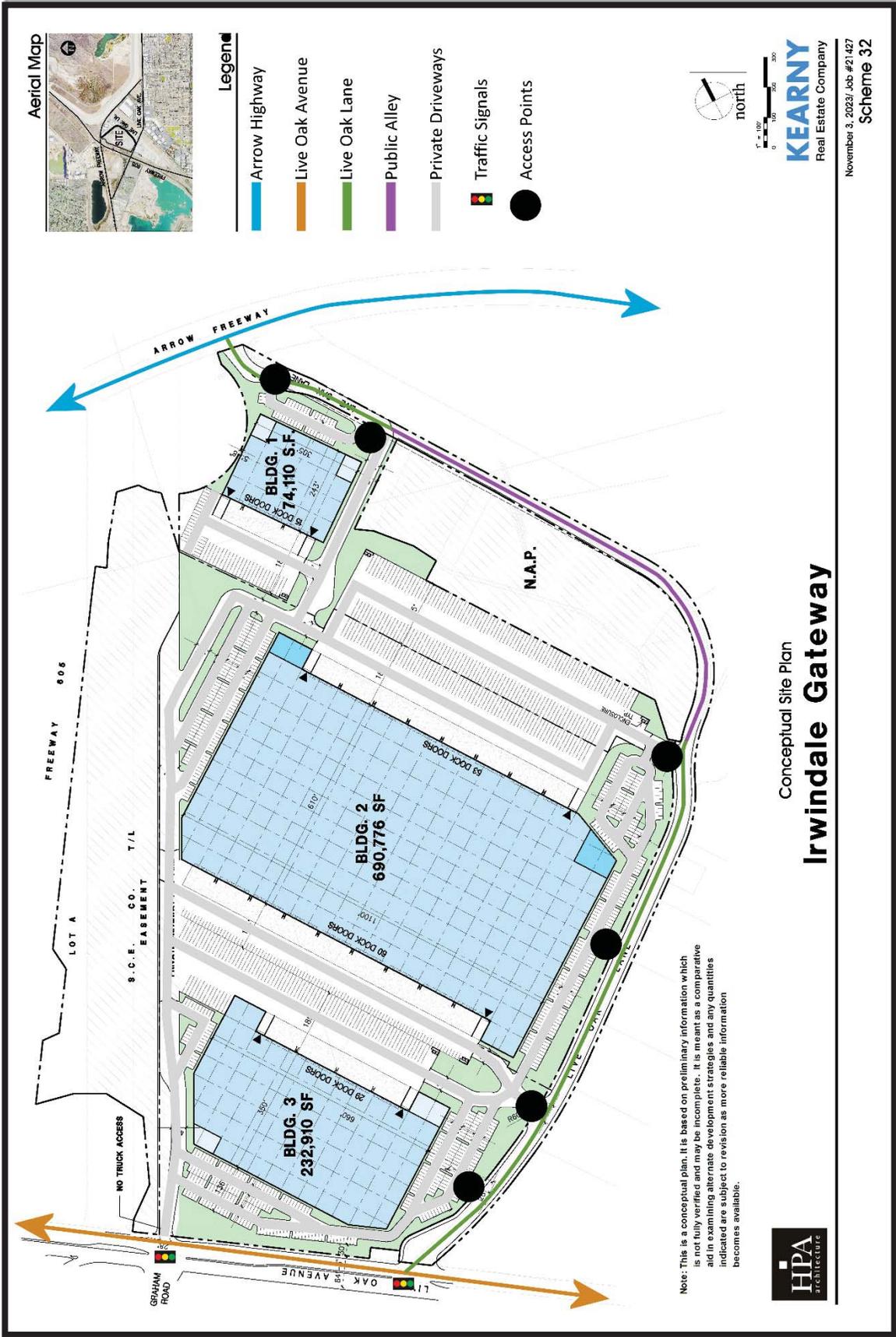


FIGURE 5-1 – CONCEPTUAL CIRCULATION

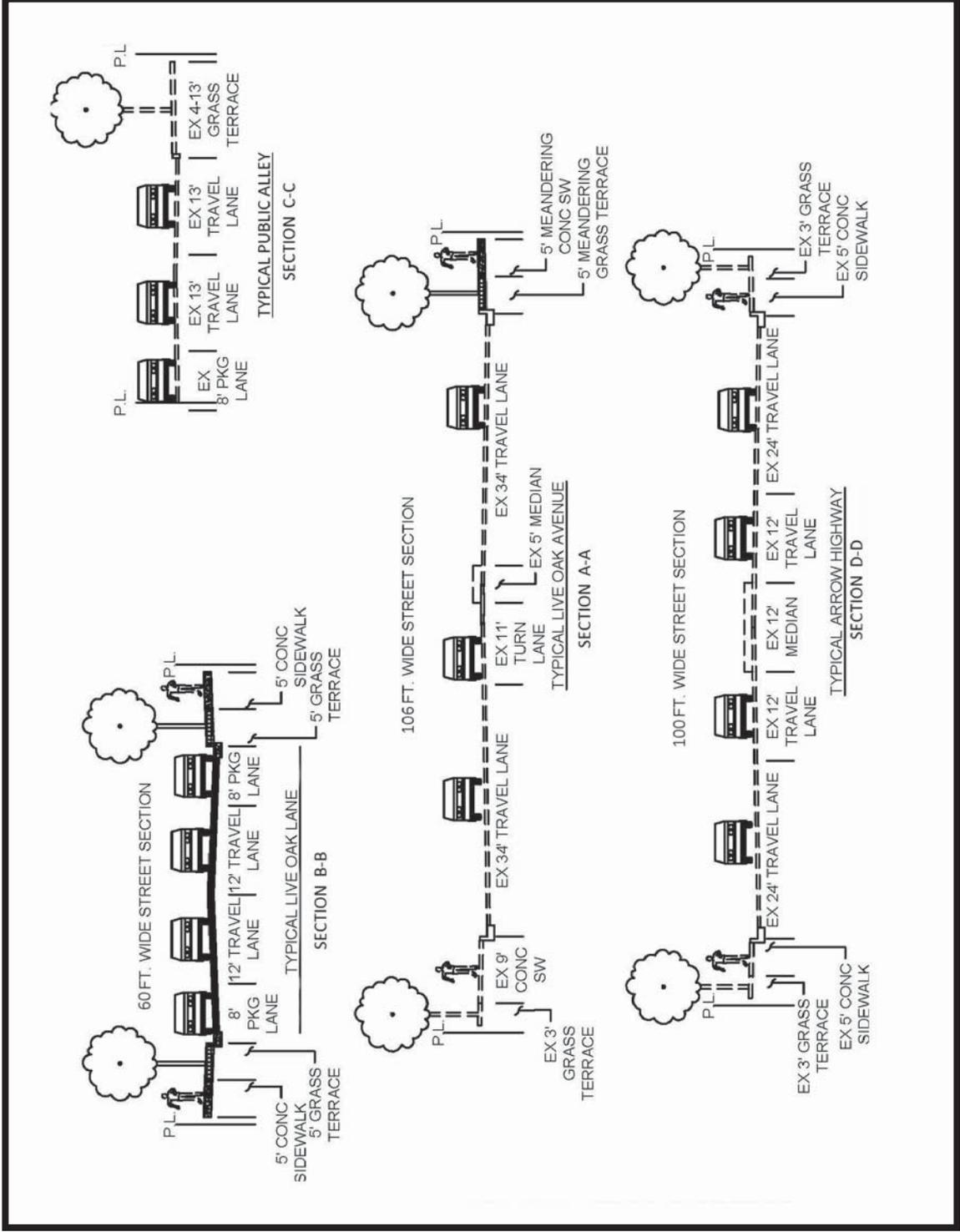


FIGURE 5-2 – TYPICAL ROW CROSS-SECTION

5.3 Non-Vehicular Circulation

The Specific Plan provides for convenient pedestrian movement and circulation within the portion of the Development Area that is accessible to the public via an integrated sidewalk network that is to be designed on individual building sites. As illustrated on 5-6 “Conceptual Non-Vehicular Access,” sidewalks are provided in the public right-of-way along the north side of Live Oak Avenue adjacent to the Development Area. Minimum 5-foot-wide sidewalks are or may be located along both sides of Live Oak Lane to facilitate pedestrian circulation between Arrow Highway and Live Oak Avenue. Crosswalks are designed at signalized intersections to ensure pedestrian safety. Foothill Transit is a public transit agency that serves the City of Irwindale. A bus stop at Live Oak Avenue and Stewart Avenue is located less than a mile from the Development Area and a bus stop at Rivergrade Road and Arrowhead Hwy is located approximately within a mile and a half of the Development Area.

Both Arrow Highway and Live Oak Avenue are Bicycle Priority Corridors in the City’s Active Transportation Plan. Development in the Development Area would not impact proposed new bicycle facilities along the roadways.

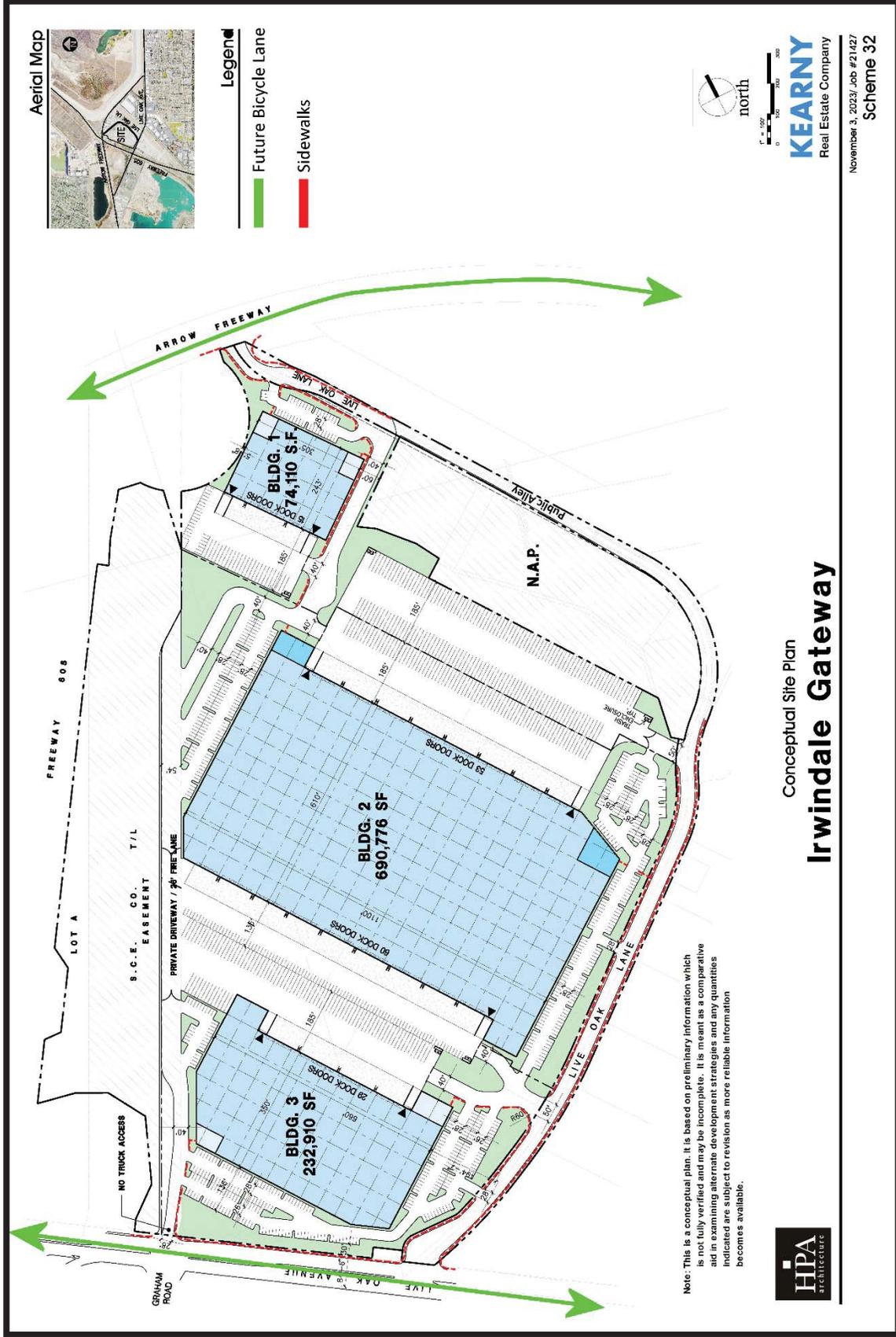


FIGURE 5-3 – CONCEPTUAL NONVEHICULAR ACCESS PLAN

Development Standards

VI.

6. Development Standards

6.1 Purpose and Intent

This section establishes development standards applicable to all developments within the Development Area. The standards provided herein work in concert with the architecture and landscape guidelines set forth in Section 7, Design Guidelines, to achieve the vision of the Specific Plan. All new developments and alterations to existing land uses and structures in the Development Area shall be designed, constructed, and established in compliance with the requirements of this Section.

6.2 Definitions

As used herein, the following terms shall have the following meanings:

“Building”: Any structure having a roof supported by columns or by walls and intended for the shelter, housing or enclosure of persons, animals or property of any kind.

“Height”: The vertical distance as measured from the highest finished grade adjacent to a Building or structure to the highest point of the same Building or structure, including any building parapet or equipment screening walls.

6.3 Permitted Uses

Uses are permitted in the Specific Plan as indicated in Table 6-1. Uses not identified in this table are not permitted unless it is determined by the Community Development Director that a use is similar to other permitted uses, pursuant to the provisions of Section 9.3.

The symbols shown in Table 6-1 are defined as follows:

- **“P”** means the use is permitted by right, subject to applicable development standards and requirements of this Specific Plan.
- **“C”** means the use is conditionally permitted, subject to the approval of a Conditional Use Permit application in accordance with the requirements of the Zoning Code.
- **“A”** means the use is permitted by right, provided the use is ancillary to the primary or conditionally permitted use of the property.
- **“DA”** means the use is subject to a Development Agreement.
- **“X”** means the use is prohibited.

Table 6-1 Allowable Uses

USE	LAND USE PLAN 1	LAND USE PLAN 2	ADDITIONAL NOTES
Professional Offices and Services			

USE	LAND USE PLAN 1	LAND USE PLAN 2	ADDITIONAL NOTES
Administrative and professional offices	P	P	
Banks and Financial Institutions	P	P	With and without drive-thru
Data processing and storage centers	P	P	
Industrial, Warehousing, Fabricating, and Manufacturing Uses			
Automobile and truck repair garages	C	C	.
Bakeries and confectionaries, including manufacturing and wholesale	P	P	
Bottling plants	P	P	
Billboards	P	P	Subject to Chapter 17.20 (Billboards) of the IMC
Computer and electronic parts manufacturing and assembly	P	P	
Concrete and concrete products manufacture	C	C	
Distribution warehouses, general warehouses, and fulfillment centers, for dry goods	P	P	
Distribution warehouses, general warehouses, and fulfillment centers for chilled, cooled, or frozen goods	P	P	A maximum of 387,500 square feet (SF) of chilled, cooled and freezer warehouse space is permitted in the Development Area.
Fabricating and machining of metal, glass, wood, stone, or paper.	P	P	Includes direct-to-consumer sales of goods and/or products on the premises, that are either manufactured, warehoused, or wholesaled on-site.
Food commissaries	C	C	
Manufacturing or processing of food	P	P	Limited to manufacture, wholesale and/or distribution only. Does not include animal slaughter.

USE	LAND USE PLAN 1	LAND USE PLAN 2	ADDITIONAL NOTES
Industrial retail sales	A	A	Limited to 15% of the Building's gross sf
Industrial robotics manufacturing and assembly	P	P	
Job printers	P	P	
Joining and assembly manufacturing	P	P	
Machine Shops	P	P	
Machinery manufacture	P	P	
Manufacturing and assembly of products made with wood, stone, fiber, textile, paper, plastic, canvas, casein, cork, clay, metal, pharmaceutical, cosmetic, electrical, glass, and wax	P	P	
Motor vehicle storage	P	P	Indoor storage only
Motor vehicle washing, cleaning, and detailing, includes mechanical or hand motor vehicle washing.	A	A	This ancillary use is not open to the public.
Plastics, fabrication from and molding	P	P	Includes the light manufacture of products thereof provided all grinding and operations are fully conducted within an enclosed Building
Plating	C	C	
Research & development services/laboratories	P	P	
Scientific instrument and equipment manufacturing or precision materials	P	P	
Self-storage/public	C	C	
Shipping/parcel delivery hub or sorting center	P	P	
Other			
Telecommunications facilities	C	C	Subject to Chapter 17.12 (Specific Use Regulations) of the IMC

6.4 Development Standards - Industrial and Business Park

Industrial and Business Park Sites shall comply with the development standards listed in Table 6-2.

Table 6-2-Development Standards

Development Standards	Requirement
Site Requirements	
Minimum Lot Size	40,000 square feet
Maximum Floor Area Ratio ¹	0.65
Minimum Setback Requirements²	
Arrow Highway Setback	
Building	20 ft.
Drive Aisle and Parking ³	12 ft.; 10 ft. adjacent to dedicated turn pockets
Live Oak Avenue Setback	
Building	20 ft.
Drive Aisle and Parking ³	12 ft.; 10 ft. adjacent to dedicated turn pockets
Live Oak Lane Setback	
Building	10 ft.
Drive Aisle and Parking ³	10 ft.
Allowable architectural encroachments into setbacks (cornices, eaves, canopies, etc.) ⁴	5 ft.
Building Requirements	
Maximum Building Height	60 ft.
Minimum Distance Between Buildings	20 ft.
Minimum Drive Aisle/Parking Space Separation	
Adjacent to building wall	10 ft.
Adjacent to screen wall/fence	5 ft.
Within screened/enclosed yards	0 ft.
Minimum Parking and Access Requirements	
Automobile Parking Ratio	Per Chapter 17.16 (Parking and Loading Standards) of the IMC
Automobile Parking Stall Size	9 ft. x 19 ft. (standard) 8 ft. x 15 ft. (compact) ⁵
Truck Parking/Storage Stall Size	10 ft. x 55 ft.
Drive Aisle Width	26 ft.
Other Development Standards	
Walls and Fences	4 ft. minimum height 14 ft. maximum height
Minimum Landscaped Area	Per Section 6.6
Site Lighting	Light fixtures shall not exceed a height of 35'. Exterior lighting shall produce a maximum initial illuminance of no greater than 0.5 horizontal and vertical footcandles at the site boundary and beyond

Development Standards	Requirement
Signs	In accordance with Chapter 17.19 (Signs) of the IMC

Notes

¹ The FAR maximum shall be calculated as a ratio of (1) the aggregate square footage of the Building(s) located on any portion of the Development Area developed for Industrial and Business Park Uses divided by (2) the total square footage of such development site minus areas of public right of way. Under Land Use Plan 1 this would result in 1,490,732 s.f. (52.65 acres x 43,560 sf/acre x 0.65) and 1,039,407 s.f. (36.71 acres x 43,560 sf/acre x 0.65) under Land Use Plan 2.

² Minimum setback area shall be that distance measured between the face of the building wall or closest side of structure to the edge of the public right of way.

³ Entire setback shall be landscaped. Landscaping shall include trees, shrubs, groundcovers, and/or vines and may include walkways, benches, trellises, thematic fencing/walls, and related amenities.

⁴ Only permitted where adequate emergency access can be maintained.

⁵ Compact spaces shall comprise no more than 25% of all required automobile parking spaces.

6.4.1 Other Development Standards (Industrial and Business Park)

In addition to the development standards required by Table 6-1, the following standards apply to Industrial and Business Park Sites:

1. Manufacturing and processing activities shall be conducted within a wholly enclosed building.
2. Outdoor storage of materials and equipment shall be ancillary to a permitted use. Outdoor storage shall be fully screened and trucks and vehicles shall be stored within enclosed areas.
3. All loading/unloading of trucks shall occur within the truck courts. Trucks and service vehicles shall have clear and convenient access into and within truck courts or loading areas of every building and shall not be located to disrupt vehicular and pedestrian circulation. The loading/unloading of trucks within the right-of-way shall be prohibited.
4. Loading docks and truck yards shall be visually screened from public streets by walls, fencing, landscaping, and/or other screening features or barriers (such as berms) with a height of 14 feet, as measured from the interior side of the barrier.
5. Ground-mounted exterior mechanical and electrical equipment, heating, ventilating, air conditioning, tanks, and other mechanical and electrical devices shall be screened and treated with a neutral color or obscured by landscaping when visible from Arrow Highway, Live Oak Avenue, or Live Oak Lane.
6. Roof-mounted equipment shall be screened when visible from Arrow Highway, Live Oak Avenue, or Live Oak Lane. Screening may be accomplished with the building parapet or the use of a roof mounted equipment screen. Screening materials shall be compatible with the design and architecture of the building.
7. Entry gates shall be positioned to allow enough distance for the stacking of at least one (1) 5-axle truck in front of each gate without any portion of the truck extending into the public right-of-way.
8. Locate and design service entrances so they do not interfere with owner/tenant/customer access.

9. Trash and refuse collection areas shall comply with the provisions of Chapter 17.13 (Site Planning and General Development Standards) of the IMC.
10. Prior to issuance of a building permit, a Comprehensive Sign Program shall be approved by the City pursuant to Chapter 17.19 (Signs) of the IMC.

6.5 Landscape

The following requirements shall apply to all site improvements within the Development Area, regardless of which Land Use Plan is implemented.

6.6.1 General Requirements

1. **Minimum Site Landscaping.** A minimum of 10% of the site area shall be landscaped.
2. **Parking Lot Landscaping.** A minimum of 20% of the total required landscape shall be provided in the parking lot.
3. **Required Areas.** All setbacks, parkways, open areas, plazas, paseos, and non-work areas that are visible from a public street/alley or from a parking lot available to the general public shall be landscaped.
4. **Landscape Coverage Requirement.** Shrubs, groundcover, and other plant material shall cover all areas that are visible from a public street/alley or from a parking lot available to the general public and that are not occupied by structures, parking areas, storage, trash enclosures, driveways, and sidewalks at the time of issuance of a Certificate of Occupancy. Embellished pavement, fountains, and similar hardscape materials may, in part, be substituted for the required landscaping through the Site Plan and Design Review process.
5. **Parkway-Adjacent Planting and Maintenance.** Parkway located between the sidewalk and the edge of development shall meet the following requirements:
 - a. The ground surface shall contain shrubs, mulch, or ground cover to provide coverage within two years.
 - b. If a wall or fence separates the development from the street, planting vines or espalier shrubs shall be incorporated into the planting design.
6. **Required Landscaping for Loading Areas.** Loading areas shall incorporate landscaping to provide screening if visible from the public right-of-way, adjacent uses, and pedestrians.

6.6.2 Trees

1. Perimeter landscaping shall include one street tree for each 30 lineal feet of street frontage.
2. Within parking areas, one tree for each 25 parking spaces shall be provided.
3. All trees planted along a street frontage shall be of a minimum 15-gallon size. Palm trees shall have a minimum brown trunk height of six feet.
4. All other trees planted in required landscaped areas shall be a minimum of five-gallon size or have a brown trunk height of three feet.

Design Guidelines

VII.

7. Design Guidelines

7.1 Purpose and Intent

The Design Guidelines presented in this section establish the quality and character of the built environment for the master-planned development of the Specific Plan. The objectives of the Design Guidelines are:

1. To describe the thematic elements and the construction quality expected for the Development Area.
2. To provide the City of Irwindale with assurance that the Development Area will be developed in accordance with the quality and character described within this Specific Plan.
3. To serve as a guide to developers, builders, engineers, architects, landscape architects, and other professionals involved with implementing development in the Development Area in order to achieve and maintain the desired design quality.
4. To provide an aesthetic benchmark for the City of Irwindale to use in their review of future implementing projects within the Development Area.
5. To steer the Development Area to convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
6. To encourage the implementation of energy efficient design features in Buildings that can be implemented in the site planning, design, and construction phases of the Development Area to minimize waste deposited at landfills, decrease energy use and fossil fuel consumption, and reduce domestic water consumption.

7.2 Applicability

The Design Guidelines presented in this chapter apply to all development within the Specific Plan Area. Where these Design Guidelines are silent, the City's Commercial and Industrial Design Guidelines shall prevail.

7.3 Site Planning

This section sets forth general site planning guidelines that address unique considerations associated with development within the Development Area.

1. Locate the office portion of Buildings at the corner(s) of the Building. For Buildings adjacent to Live Oak Avenue or Live Oak Lane, orient the office toward the road to provide visual interest from the public roadway.
2. Orient and screen elements such as trash enclosure areas, loading bay doors, and service docks in ways that minimize their visibility from Live Oak Avenue or Live Oak Lane.
3. Use solid walls, screened fences, landscaping, or other visual barriers to visually screen truck courts and loading docks from public view from Live Oak Avenue or Live Oak Lane, where possible.
4. Site design shall specifically address the needs of pick-up, delivery, and service vehicles related to Industrial and Business Park Uses.

- a. Design loading areas to provide for tractor trailer backing and maneuvering on-site and not from a public street.
- b. Provide appropriate on-site service vehicle parking/turnouts in an efficient, non-obtrusive location appropriate to the scale and needs of the development.
- c. Loading vehicles, when parked, shall not impede normal traffic flow.

Conceptual site plans are shown in Figures 7-1 and 7-2.

7.4 Building Architecture

7.4.1 Architectural Design

Buildings should be characterized by simple and distinct cubic masses with interlocking volumes of wall planes, colors, and materials to create visual appeal, aesthetically pleasing proportions, and strong shadow patterns. Colors, materials, and textures will be mixed to create interest.

A Spanish contemporary influence is required throughout the Development Area. All design elements of Buildings shall be compatible (but not identical) in character, massing, and materials in order to promote a clean and contemporary style. Creativity is encouraged in building design, with care taken to maintain a sense of similarity among all Buildings to reinforce a unified image within the Development Area. Generally, Buildings within the Development Area shall not be overly “trendy” or strongly historical; however, contemporary Spanish thematic elements visible from public streets, such as arched entryways and windows, and other subtle references to the history of the region are acceptable. Architectural styles should complement, not detract from, the general architectural character found in the City of Irwindale.

7.4.2 Building Form

Building form is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a Building’s “form.” Building forms are especially important for building elevations that face the following view corridors:

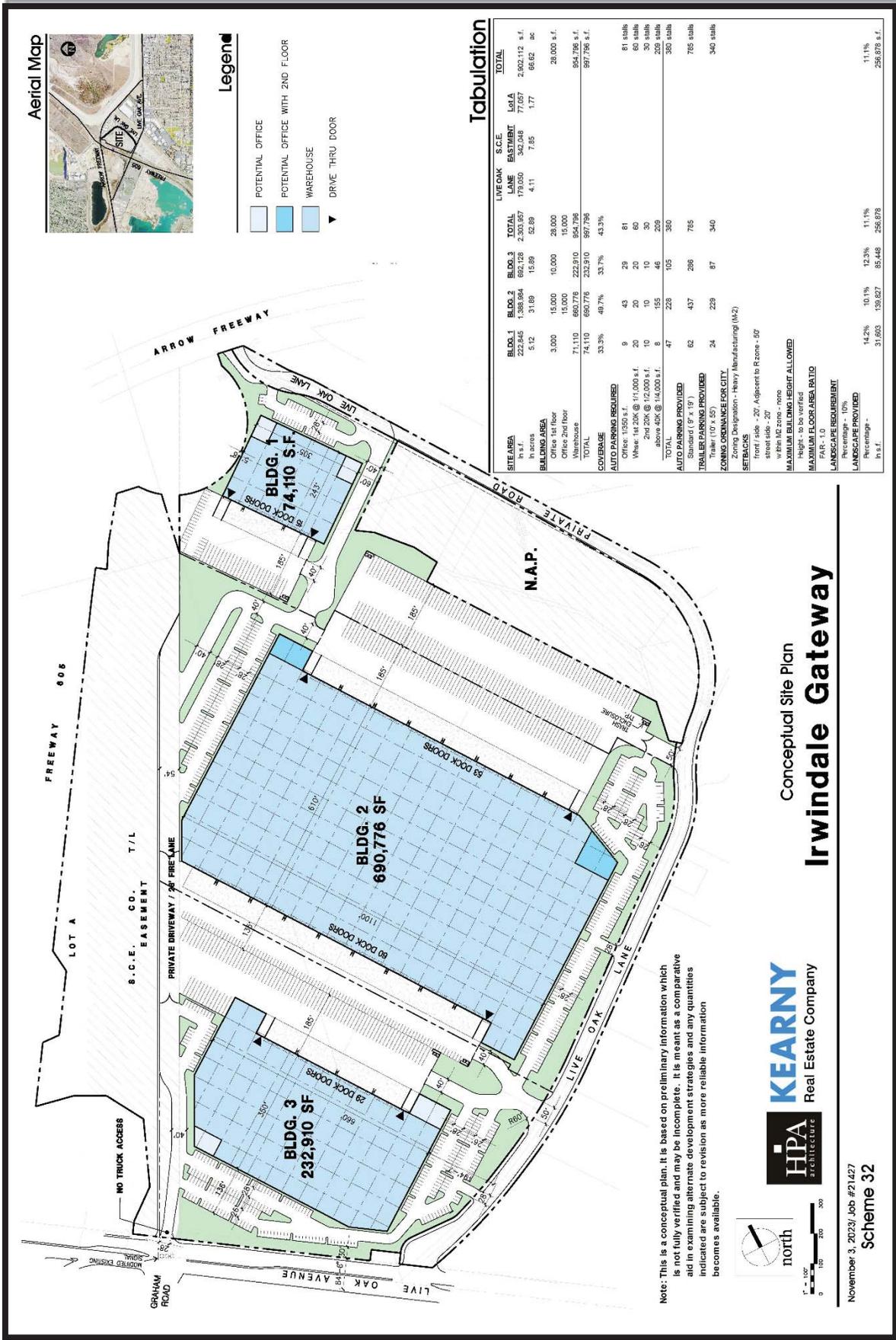
1. Building façades that face and are immediately adjacent to and visible from Live Oak Avenue.
2. Building façades that face and are immediately adjacent to and visible from Arrow Highway.
3. Building façades that face and are immediately adjacent to and visible from Live Oak Lane.

The following guidelines apply to all Buildings within the Development Area to ensure that structural development is visually appealing and inviting to pedestrians and motorists:

1. Use geometric forms to constitute the overall building form. Rectangular forms are encouraged to promote balance, rhythm, and visual interest. Layering of forms creates detail, depth and shadow and is strongly encouraged. However, avoid arbitrary, complicated building forms.
2. Articulate building planes visible from Live Oak Avenue, Arrow Highway, I-605, and/or Live Oak Lane by changes in exterior building materials, color, texture, and decorative accents. Articulated features (e.g., pop-outs and recesses, breaks, overhangs, height changes, etc.) should be used to create a pedestrian scale at primary Building entries.

3. Modulation and variation of building masses between adjacent Buildings visible from Live Oak Avenue, Arrow Highway, and Live Oak Lane is encouraged. This includes varied parapet heights and change of building plane, in plan.
4. Generous use of windows and doors in the office areas and articulation and different paint colors to break up Building walls and bulk.
5. Design each Building to have a well-defined entry with careful roof and façade articulation to create visual interest and scale, including changes in massing, color, and/or building materials.
6. Recess or cover pedestrian and ground-level Building entries by architectural projections or roofs in order to provide shade and visual relief.
7. Design Buildings to have a base and cornice expression. These expressions can be accomplished through the use of clean, simplistic, and not overly complicated architectural and trim detailing on Building façades, and changes of material/color or recesses.
8. Materials applied to building elevations should turn the corner of the Building to a logical termination point in relation to architectural features or massing.

Conceptual elevations demonstrating acceptable building form are illustrated in Figure 7-3 below.



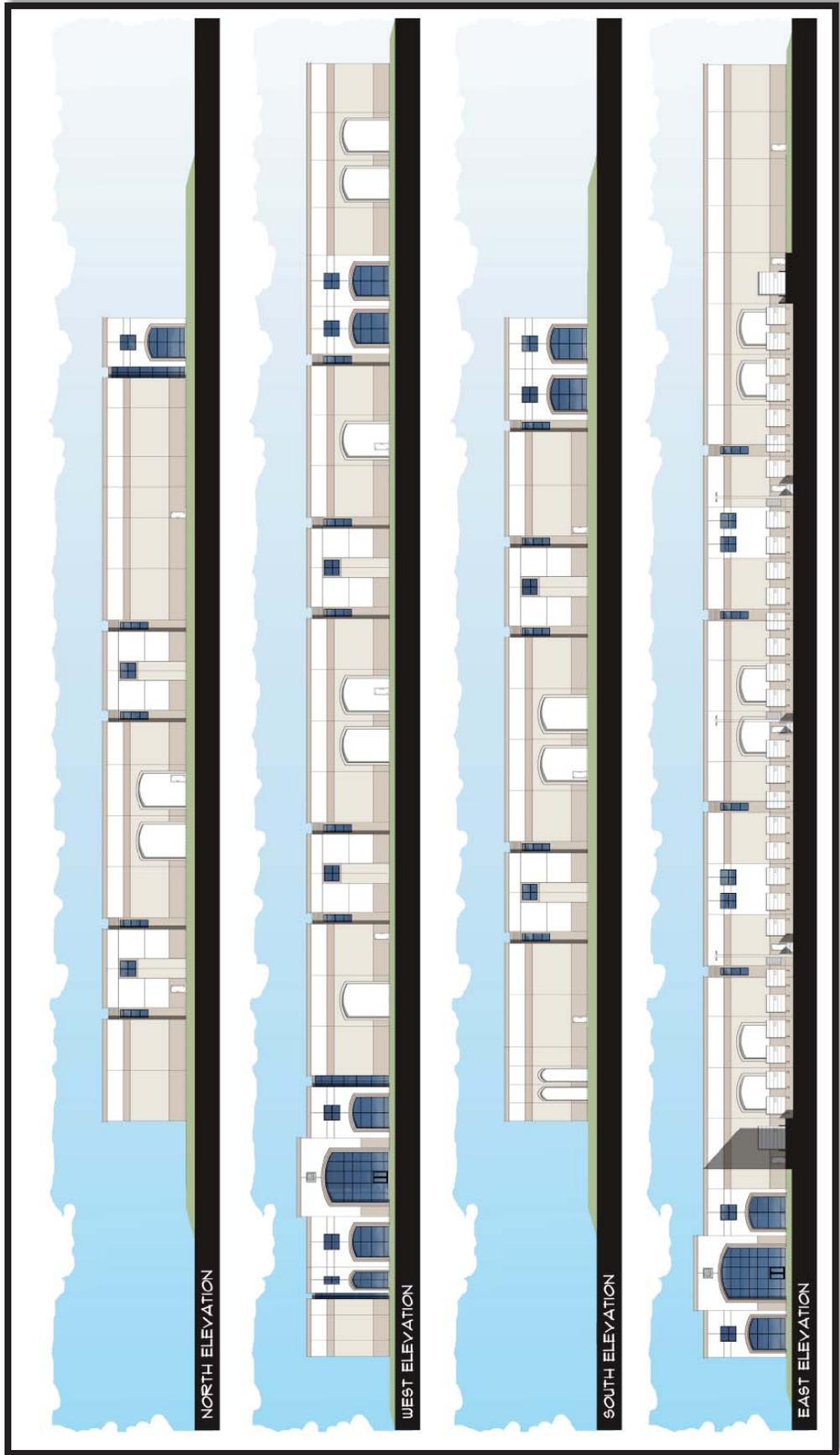


FIGURE 7-2 – CONCEPTUAL BUILDING FORM

7.4.3 Building Materials, Colors, and Textures

The use of complementary materials and colors for Buildings plays a key role in developing a clean, contemporary visual environment. Accordingly, the selected exterior materials, colors, and textures for Buildings should complement one another among all Buildings within the Development Area. Slight variations in materials, colors, and/or textures from Building to Building are encouraged to provide visual interest.

1. Appropriate primary exterior building materials include stucco, concrete, and similar materials, including concrete tilt-up panels. Accent primary materials used on Building facades that are visible from public rights of way through the use of secondary materials such as glass or glazing units, glass block, natural or fabricated stone, brick, metal, and tile or tile panel systems.
2. The use of metal and/or glass fabrications for curtain wall areas are appropriate.
3. Trim details may include metal finished in a consistent color, plaster, contemporary shaped foam, or concrete elements finished consistently with the building treatment. Use of overly extraneous “themed” detailing, like oversized or excessively shaped foam cornice caps, foam molding, and window detailing is discouraged.
4. Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
5. Primary exterior Building colors are encouraged to be light and warm tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around Building entrances and near outdoor gathering spaces.
6. Bright primary colors, garish use of color and arbitrary patterns or stripes are discouraged, except in signage logos.
7. Exposed downspouts, service doors and mechanical screen colors should be the same color as the adjacent Building wall.

The Suggested Material and Color Palette shown in Figure 7-4 below is for conceptual purposes only. Other colors that are consistent or similar in nature to the colors provided in the Suggested Color Palette may also be considered as appropriate.

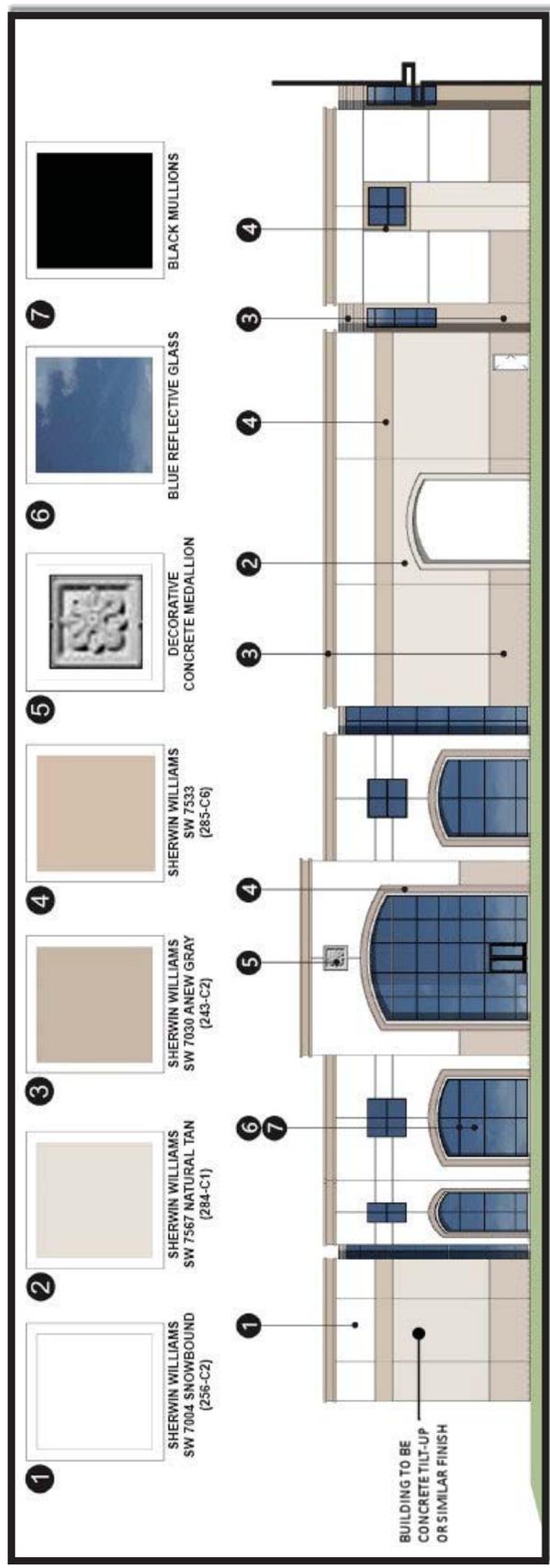


FIGURE 7-3 – COLOR AND MATERIAL PALETTE

7.4.4 Windows and Doors

The patterns of openings – windows and doors (excluding trailer-loading dock doors) – are to correspond with the overall rhythm of the Building and be consistent in form, pattern, and color. Guidelines for windows and doors used in the Development Area are as follows:

1. Introduce recessed window and door openings to enhance the visual play of light and shadow.
2. When possible, place the layout of doors and windows on individual Building façades in a repetitive pattern to create continuity.
3. Use consistent styles, forms and colors of windows.
4. Gold or unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.
5. Use clear or colored glass with medium to high performance glazing. Silver mirrored glass is prohibited.
6. Clearly define all pedestrian entrances to Buildings by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways, and unarticulated entry alcoves are discouraged.

Figure 7-4- Windows and Doors



7.4.5 Functional Elements

Carefully consider the design and location of functional elements common to all Buildings. Examples of functional elements include loading doors, service docks, ground or wall-mounted equipment, rooftop equipment, rain gutters and downspouts, and trash enclosures.

Loading Doors and Service Docks

1. Orient and screen loading doors, service docks, and equipment areas so they are not easily visible from Live Oak Avenue, Arrow Highway, Live Oak Lane, and publicly accessible locations within the Irwindale Gateway Development Area. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the Building or by any effective combination of walls, fences, landscaping, and berms.
2. No direct loading or unloading activity is permitted to take place from Live Oak Avenue, Arrow Highway, or Live Oak Lane. Trucks and service vehicles shall have clear and convenient access

into and within truck courts or loading areas of every building within the Development Area and should not disrupt vehicular and pedestrian circulation.

3. Separate loading docks and truck courts from visitor and customer parking areas and pedestrian circulation areas (e.g., walkways) utilizing walls, fences and/or landscaping.
4. Design truck and service vehicle entries to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected.

Ground or Wall-Mounted Equipment

1. Locate electrical equipment rooms within a Building. Pop-outs or shed-like additions are discouraged, unless fully incorporated in the architectural concept of the Building.
2. Ground-mounted equipment, including but not limited to mechanical equipment, electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, but specifically excluding electrical transformers, shall be screened to limit visibility from off-site public viewing areas. Screening may be accomplished with site walls or landscape elements that are consistent with these Design Guidelines.
3. Wall-mounted items, such as roof ladders or electrical panels, shall not be located on the Building façade facing adjacent public roads when alternative locations are practical and safe. Wall-mounted items should be screened or incorporated into the architectural elements of the Building so as not to be visually obvious from public streets or other publicly accessible areas within or adjacent to the Development Area.

Rooftop Equipment

1. Rooftop equipment, including but limited to mechanical equipment, electrical equipment, storage tanks, cellular telephone facilities, satellite dishes, skylights, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened to limit visibility from public roads or visitor parking lots within the Development Area.
2. Rooftop screens (i.e., parapet walls) shall be integrated into the architecture of the main Building.
3. Wood finished rooftop screens are prohibited.

Trash Enclosures

1. All outdoor refuse containers shall be screened within a permanent, durable enclosure and should be oriented so they are not easily visible from public roads or other public viewing areas.
2. The design of trash enclosures shall reflect the architectural style of adjacent Buildings and use similar, high-quality materials.
3. Refuse collection areas shall be located behind or to the side of Buildings, away from the Building's main entrance.
4. All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted storm water runoff from these structures.

7.5 Landscape Architecture

The Specific Plan incorporates landscaping as a visual amenity along its perimeter to soften and minimize the perceived scale of development. Figures 7-6 and 7-7 illustrate the location of landscape amenities around the Development Area.

These Landscape Design Guidelines complement the existing setting of Irwindale, Southern California climate and local soil conditions, ease of maintenance, and water conservation. Water-efficient and drought-tolerant plant materials shall be placed throughout the Specific Plan and 'smart' computer-controlled irrigation systems shall be used to reduce water use to the minimum level necessary. These Landscape Design Guidelines promote an identity for the Development Area that is visually appealing and sensitive to the environment.

Although specific design information is presented in these Landscape Design Guidelines, these Guidelines are not intended to establish a set of rigid landscaping requirements for the Development Area and it is recognized that, at times, there may be a need to adapt these guidelines to meet certain pad-specific or building user identity requirements. As such, these Landscape Guidelines are intended to be flexible. However, it is critical to the Development Area's long-term design integrity that any deviations from these Landscape Design Guidelines are in keeping with the spirit of the core elements of the overall theme described herein to ensure a cohesive and unified landscape concept across the Development Area.

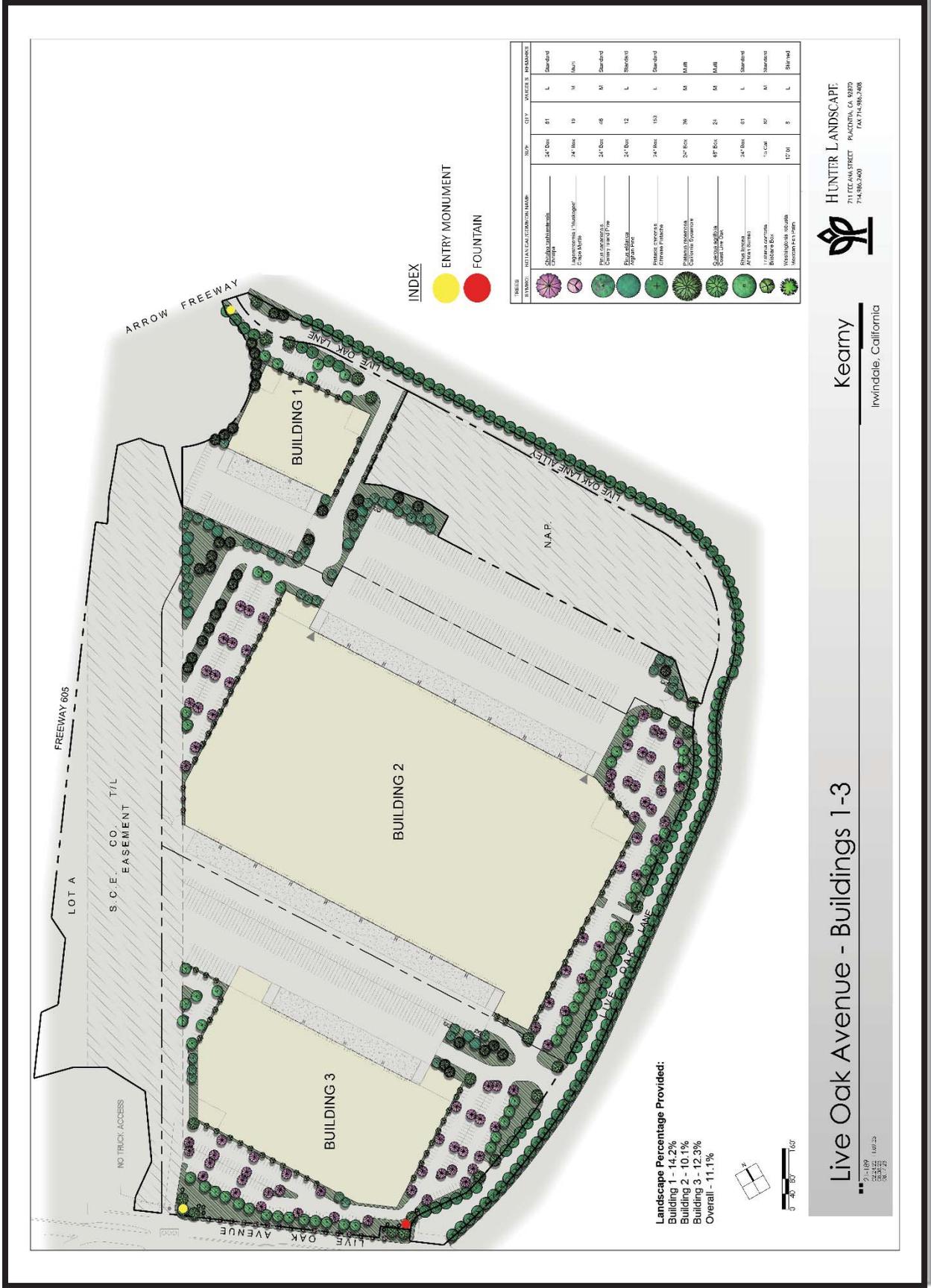


FIGURE 7-5 – CONCEPTUAL LANDSCAPE PLAN WITHOUT BESS

7.5.1 Plant Palette

The plant palette for the Specific Plan includes colorful shrubs and groundcovers, ornamental grasses and succulents, and evergreen deciduous trees – including flowering varieties – that are commonly used throughout southern California, and which complement the Specific Plan’s design theme and setting. The plant materials used within the Specific Plan are water-efficient species that can thrive in the arid southern California climate.

Table 7-1, Plant Palette, provides a list of plant materials approved for use within the Specific Plan. The plants listed in Table 7-1 establish a base palette for the Specific Plan’s landscape design. Other similar plant materials may be substituted for species listed in Table 7-1, provided the alternative plants are drought-tolerant and complement the Specific Plan’s landscape theme.

Table 7-1-Plant Palette

PLANT TYPE	PLANTS
ENTRY ACCENT PALM TREES	Date Palm (<i>Phoenix dactylifera</i>)
STREET TREES	Camphor Tree (<i>Cinnamomum camphora</i>) Carrotwood Tree (<i>Cupaniopsis anacardioides</i>) Jacaranda (<i>Jacaranda mimosifolia</i>) London Plane Tree (<i>Platanus x acerifolia</i>) Ornamental Pear (<i>Pyrus calleryana</i>) Queen Palm (<i>Syagrus romanzoffiana</i>)
PARKING LOT TREES	African Sumac (<i>Searsia lancea</i>) Bottle Tree (<i>Brachychiton populneus</i>) Fern Pine (<i>Podocarpus gracilior</i>) Karee (<i>Rhus lancea</i>)
FLOWERING TREES	Blue Palo Verde (<i>Parkinsonia florida</i>) Western Redbud (<i>Cercis occidentalis</i>) Chinese Flame Tree (<i>Koelreuteria bipinnata</i>) Pink Dawn (<i>Chitalpa tashkentensis</i>) Crape Myrtle (<i>Lagerstroemia</i>)
EVERGREEN TREES	Australian Willow (<i>Geijera parviflora</i>) Brisbane Box (<i>Lophostemon confertus</i>) Coast Live Oak (<i>Quercus agrifolia</i>) Mondell Pine (<i>Pinus eldarica</i>)
SHRUBS	Little John (<i>Callistemon</i>) Coastal Rosemary (<i>Westringia fruticosa</i>) Purple leaf hopbush (<i>Dodonaea viscosa ‘purpurea’</i>) Fortnight Lily (<i>Dietes bicolor</i>)

PLANT TYPE	PLANTS
	Indian Hawthorn (<i>Rhaphiolepis indica</i>) Waxleaf Privet (<i>Ligustrum japonicum 'texanum'</i>) Mock Orange (<i>Pittosporum Tobira</i>) Tuscan Blue Rosemary (<i>Rosmarinus o. 'tuscan blue'</i>) Texas Sage (<i>Leucophyllum frutescens</i>)
GROUND COVERS	Desert Carpet (<i>Acacia redolens</i>) Autumn Sage (<i>Salvia greggii</i>) Deer Grass (<i>Muhlenbergia rigens</i>) Small Cape Rush (<i>Chondropetalum tectorum</i>) Dwarf Mat Rush (<i>Lomandra longifolia 'breeze'</i>) Dwarf Yellow Bush Lantana (<i>Lantana m. 'dwarf yellow'</i>) Mexican Sage Bush (<i>Salvia leucantha</i>) Star Jasmine (<i>Trachelospermum jasminoides</i>) Creeping Rosemary (<i>Rosmarinus o. 'prostratus'</i>) Tall Fescue (<i>Festuca arundinacea</i>)

7.5.2 Entry Treatments and Project Identification

Project Monuments

Three (3) project monuments (signs and water features) are provided to identify the Development Area. A monument sign fronting Live Oak Avenue across from Graham Avenue will provide identification on the south end of the Development Area (see Figures 7-8 and 7-9). This monument will be highly visible from the I-605 Live Oak offramp. A second monument sign at the project entry on Live Oak Lane at the intersection with Arrow Highway will provide identification on the north end of the Development Area (see Figures 7-10 and 7-11). A fountain at Live Oak Lane and Live Oak Avenue will provide an attractive welcome as motorists and pedestrians enter the Development Area (See Figure 7-12). The entry treatments are designed to provide distinctive visual statements and emphasize the Development Area's contemporary aesthetic.

The project monuments described and illustrated herein are designed to provide a strong sense of arrival to employees, visitors, and passing motorist, to identify the distinctiveness of the Development Area, and to complement and reinforce the Development Area's general architectural and landscape theme. Implemented entry treatments may differ slightly from the concepts presented herein; however, all corner entry treatments provided within the Development Area shall be consistent in theme and character. The designs of these entry treatments are conceptually shown below. Each location provides a monument sign and landscaping consisting of water features, accent palm trees, drought tolerant ground cover and shrub masses, screen shrubs, and street trees. Flowering and colorful plant material is recommended.

Building Site Entry Treatments

Entry treatments for building sites may be provided at driveways connecting to Live Oak Avenue or Live Oak Lane. The locations of such driveways will be determined at the time Buildings are designed and oriented in the Development Area as part of implementing development projects. Building entry treatments are meant to identify Building occupants and welcome employees and visitors to the site. The designs of typical Building entry treatments are conceptually shown below, and may include signs, flowering accent trees, drought tolerant groundcover and shrub masses, evergreen screen trees, and enhanced Building entry paving.

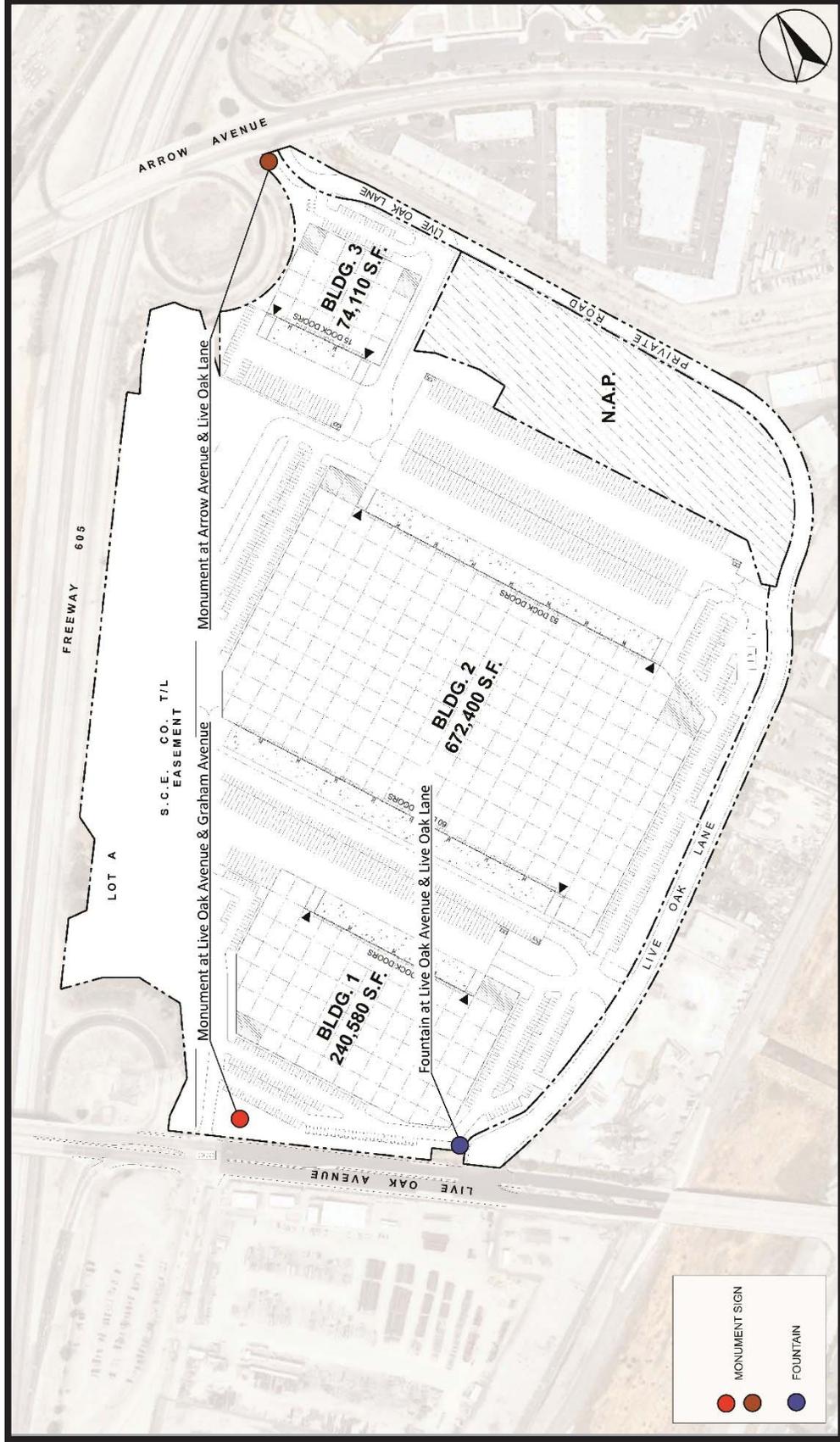


FIGURE 7-6 – ENTRY TREATMENT LOCATION MAP



FIGURE 7-9 – ENTRY TREATMENT - LIVE OAK AVENUE AT GRAHAM AVENUE

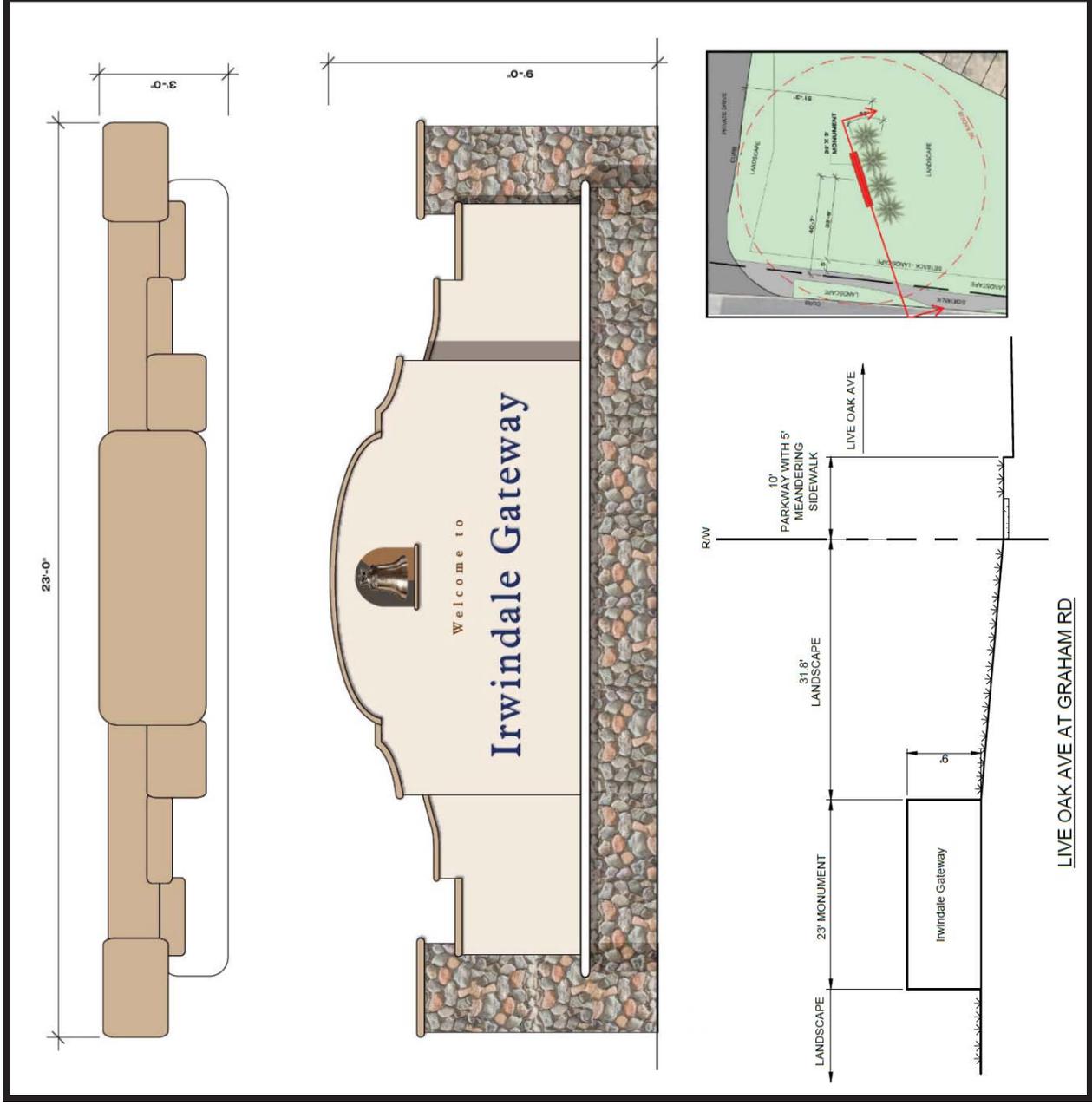
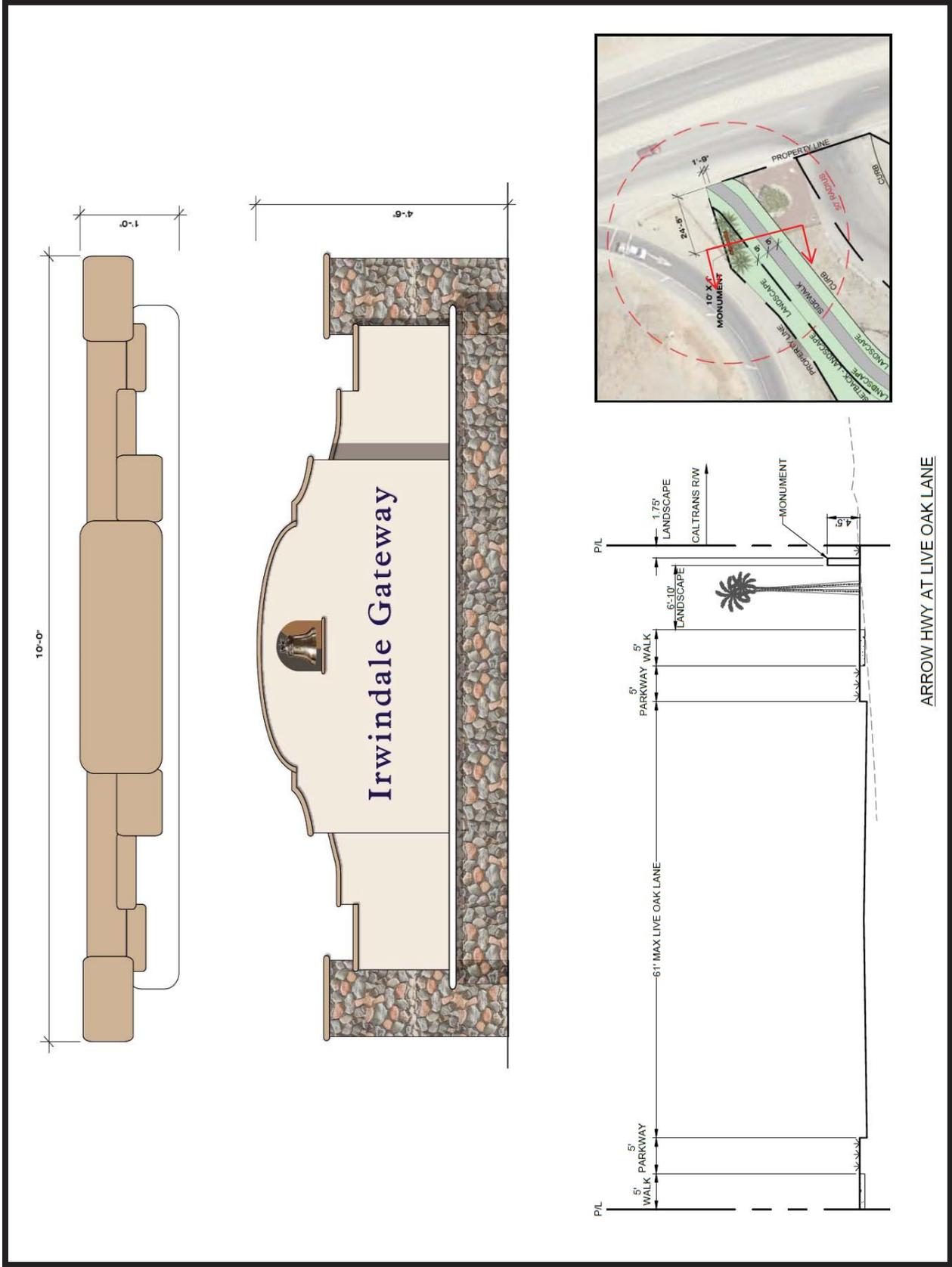


FIGURE 7-7 – ENTRY TREATMENT - LIVE OAK AVENUE AT GRAHAM AVENUE (DETAIL)



FIGURE 7-8 – ENTRY TREATMENT – ARROW HIGHWAY AT LIVE OAK LANE



ARROW HWY AT LIVE OAK LANE

FIGURE 7-9 – ENTRY TREATMENT – ARROW HIGHWAY AT LIVE OAK LANE (DETAIL)

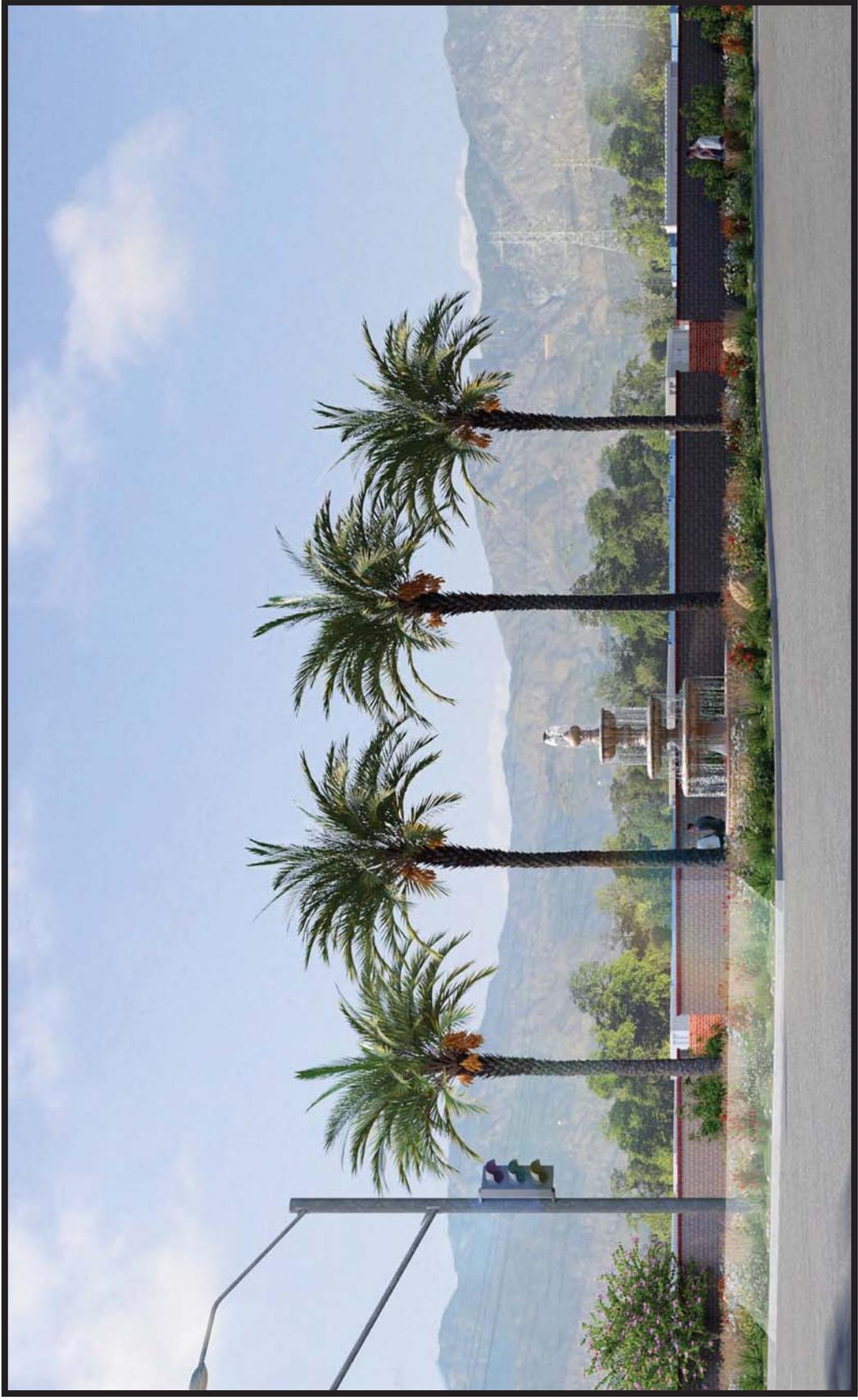


FIGURE 7-10 – ENTRY TREATMENT LIVE OAK AVENUE AT LIVE OAK LANE

7.5.3 Streetscapes

Streetscape landscaping within the Specific Plan plays a critical role in establishing a strong sense of place and character. In addition, streetscapes serve functional purposes, including screening undesirable functional elements of a building site from public view.

Live Oak Avenue Streetscape

The southern boundary of the Specific Plan abuts Live Oak Avenue. The interface with Live Oak Avenue is designed to feature a five-foot wide parkway with street trees and a five-foot wide sidewalk between the parkway and property line (see Figure 7-13). A backdrop of trees along parking lot perimeters, and screen shrubs along with assorted drought tolerant groundcovers is proposed to provide a physical and visual buffer from Live Oak Avenue. At this interface, an assortment of evergreen and deciduous canopy street trees along with palm tree clusters and screen shrubs is expected.

Live Oak Lane Streetscape

The eastern boundary of the Specific Plan abuts Live Oak Avenue. Within the Specific Plan, the streetscape of Live Oak Lane is planted with a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists (see Figure 7-14). A five-foot wide parkway with street trees and a five-foot wide sidewalk between the parkway and property line is also proposed. The landscaping plant palette for the streetscape should link the roadway to the rest of the Development Area and should reflect the Development Area's landscape design theme.

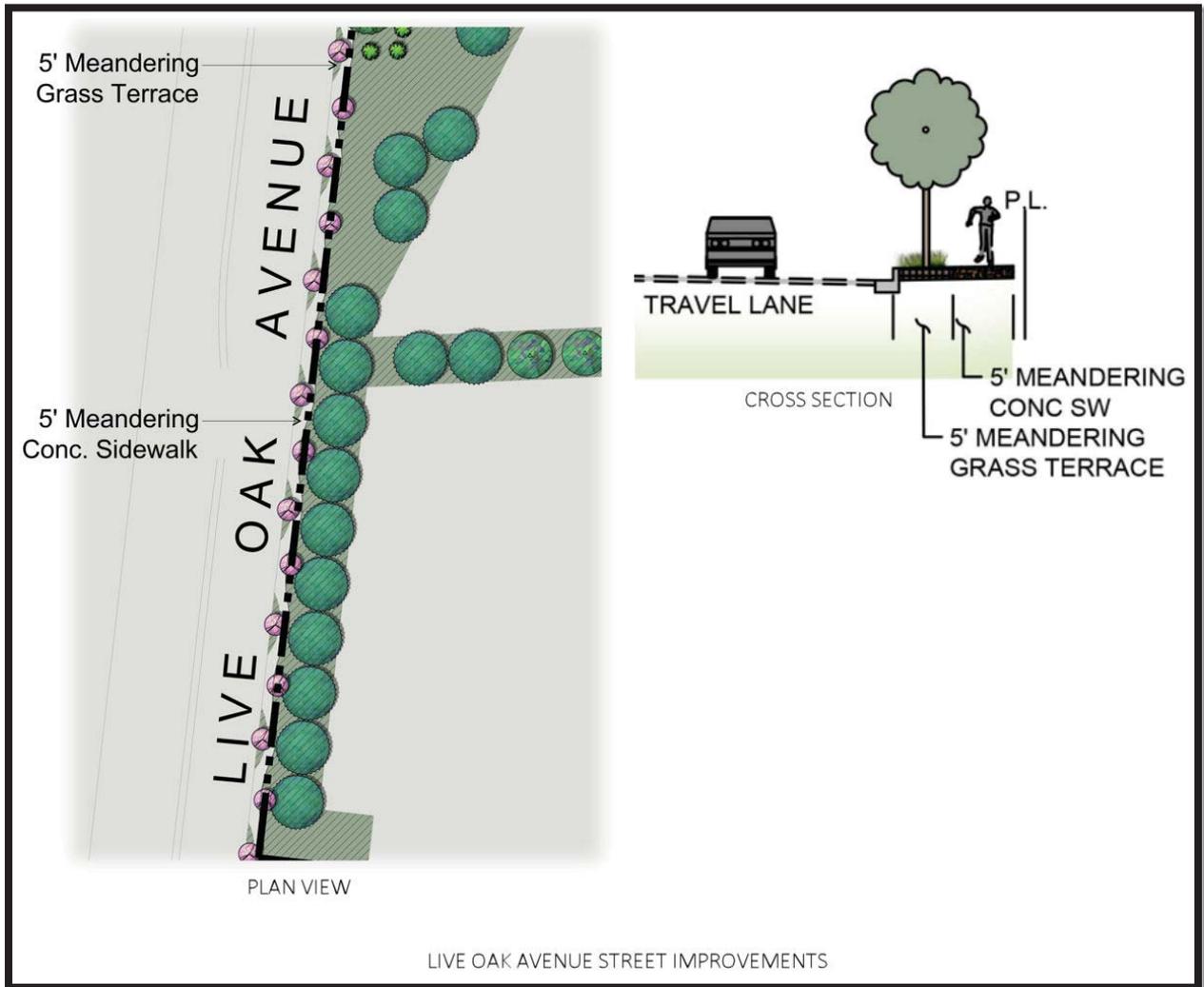


FIGURE 7-12 – STREET IMPROVEMENTS – LIVE OAK AVENUE

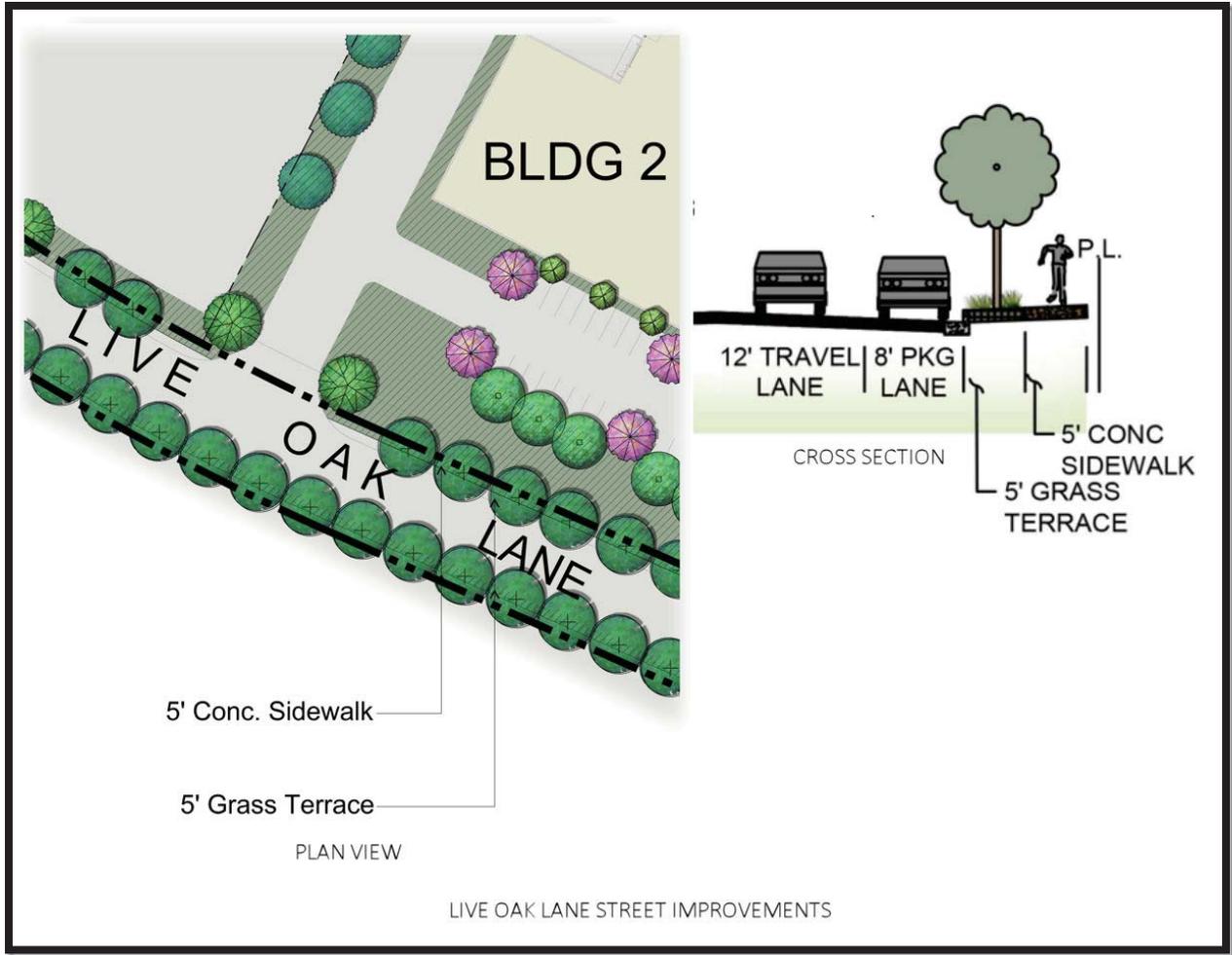


FIGURE 7-13 – STREET IMPROVEMENTS – LIVE OAK LANE

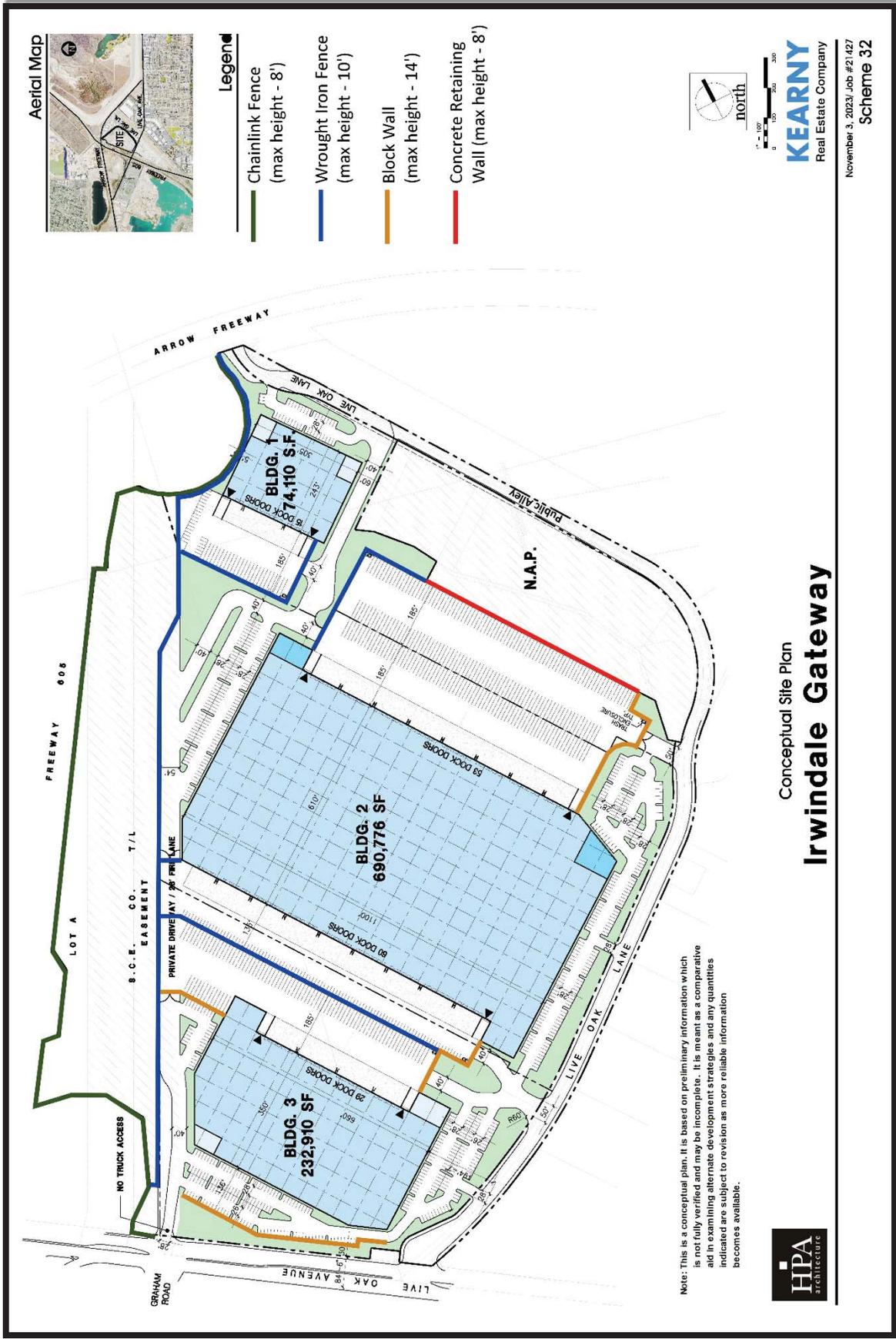
7.6 Fences and Walls

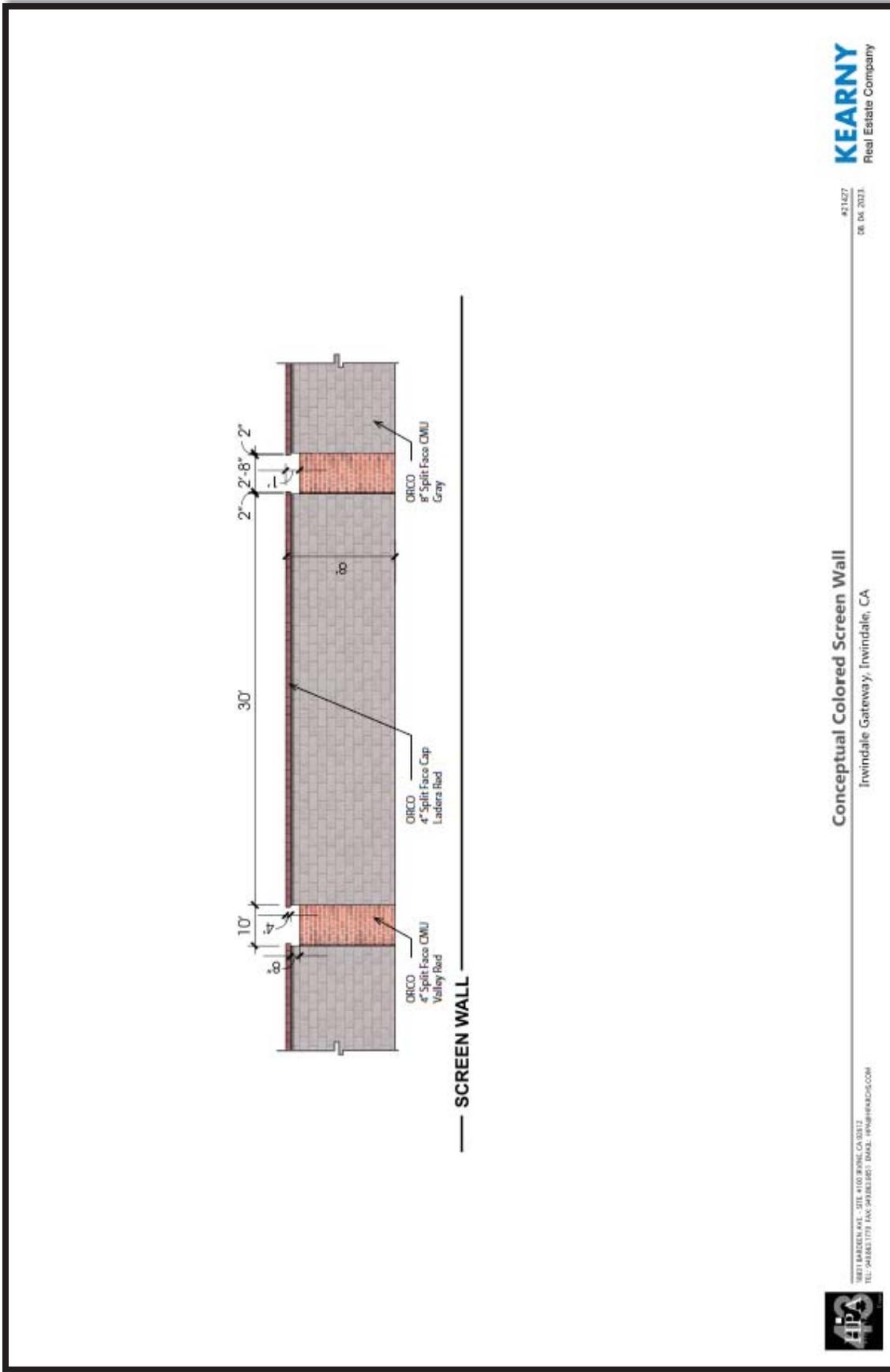
The final locations and details of walls and fences will be determined during implementation of the Specific Plan when building footprints and orientations are known. Implementing projects may modify the locations and/or details of the walls and fences described below and illustrated in Figures 7-15, 7-16, and 7-17, provided that any modifications are consistent with the spirit and intent of these guidelines.

The existing 6-foot-tall chain link fence along the western border of the Specific Plan abutting I-605 will remain in place. Tubular steel fencing is expected within individual building sites around loading and dock areas, truck yards, and surface detention basins. Tubular steel fencing is designed to range from 4 to 10 feet in height, consisting of tubular steel pickets.

Solid screen walls may be provided within individual building sites around loading and dock areas, truck yards, and parking lots. A solid wall is preferred over fencing when complete visual screening is necessary, or for noise attenuation. Screen walls are designed to be a height of 14 feet tall, as measured from the high side of the wall. Screen walls may be constructed of a decorative masonry, decorative CMU block, or concrete panel; provided, however, that a screen wall of decorative CMU block approximately 620' long shall be located along the southern boundary of the Development Area to screen the area from Live Oak Avenue. Landscaping, including trees, should be planted along the outer face of the screen wall (i.e., facing Live Oak Avenue or Live Oak Lane) to minimize the perceived size and scale of the wall.

The locations and details of walls and fences internal to the Development Area will be determined in conjunction with development of each site. Internal walls and fences may be provided along the perimeter of parking and loading areas and between building pad areas for screening and security. All internal walls visible from public streets or public viewing areas shall be built with attractive, durable materials that are compatible with other design elements of the Specific Plan.





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Conceptual Colored Screen Wall
Irvine Gateway, Irvine, CA

KEARNY
Real Estate Company

#21427
08.04.2023

FIGURE 7-15 –WALL PLAN DESIGN

7.7 Lighting

Outdoor lighting of the Development Area is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting within the public right-of-way shall adhere to applicable City of Irwindale requirements.

All lighting on private property in the Development Area should adhere to the following guidelines:

1. Minimize glare and “spill over” light onto public streets and adjacent properties by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, parking, loading, unloading, and similar areas. Where desired, illuminate trees and other landscape features by concealed light fixtures. Limit light spillover or trespass to one-quarter foot-candle or less, measured from within five feet of any adjacent property line.
2. Exterior lighting shall produce a maximum initial illuminance of no greater than 0.5 horizontal and vertical footcandles at the site boundary and beyond.
3. The maximum height of free-standing, outdoor light fixtures shall be 35 feet. The maximum height for outdoor bollard-type lighting shall be four (4) feet. Overall, light fixtures shall be the minimum height necessary to maintain pedestrian and motorist safety and facilitate site operations and security.
4. Select all lighting fixtures used in the Development Area from the same – or complementary – family of fixtures with respect to design, materials, fixture color, and light color.
5. Lights should be unbreakable, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
6. Neon, flashing and gyrating lighting are prohibited.
7. Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.
8. Illuminate parking lots, loading dock areas, pedestrian walkways, Building entrances, and public sidewalks to the level necessary for Building operation and security reasons. Dimmers and motion detectors are permitted. Lighting shall have automatic shut off features between dawn and dusk.
9. Use exterior lights to accent entrances, plazas, activity areas, and special features.
10. Provide for illumination intensity during hours of darkness as follows:
 - a. To illuminate parking lots or parking structures and their pedestrian links that provide more than five parking spaces for use by the general public, provide a minimum coverage of one foot-candle of light with a maximum of eight foot-candles on the parking or walkway surface, unless otherwise approved by the City of Irwindale for visibility and security.
 - b. To illuminate aisles and passageways within a Building complex, provide a maximum of one-half to one foot-candle of maintained lighting.
11. Low intensity, energy-conserving night lighting is preferred, such as fixtures equipped with light emitting diodes (LED).
12. High Pressure Sodium (HPS) light fixtures are prohibited for site lighting.

Utility Infrastructure Plan

VIII

8. Utility Infrastructure

Buildout of the Specific Plan requires the installation of water, sewer, storm water drainage dry utility infrastructure, and solid waste disposal as described below. All utility infrastructure improvements are required to be installed in accordance with applicable City of Irwindale and public service provider design standards and specifications.

8.1 Water

As shown on Figures 8-1 and 8-2, Conceptual Water Plan, Valley County Water District provides water infrastructure to service the Specific Plan area. An existing 12" Valley County Water District main is located in Live Oak Lane, a private road. To service the Development Area with domestic water, including fire protection service and irrigated landscaping, a connection would be made to the existing line to service future buildings and facilities. Water and fire service would be provided with a new 12-inch main comprising private on-site loop system that connects to the existing water line in Live Oak Lane. Alternatively, the water district may require new infrastructure to loop its system through Live Oak Lane. All water service and connection to the distribution system shall be reviewed and approved by the Valley County Water District.

8.2 Sanitary Sewer

As shown on Figures 8-3 and 8-4, Conceptual Sewer Plan, the Specific Plan provides private sewer infrastructure for the interior of the Development Area. Connections to existing public sewer mains are located off-site within Live Oak Avenue to the southeast of the Development Area. The sanitary sewer system for the Development Area will utilize 6"-8" on site private sewer lines, which will be conveyed to a proposed public 6" force main sewer line that drains east on Live Oak Avenue to the existing 10" VCP sewer line on Live Oak Avenue and Rivergrade Road. The private sewer infrastructure collects wastewater flows from the Development Area and conveys these flows to the off-site existing public sewer mains within Live Oak Avenue. All private sewer infrastructure will be installed on-site beneath private driveways and drive aisles, and/or parking lots/truck courts to facilitate access for routine maintenance and/or repair. Locations and alignments of all sewer mains, laterals and connection points shall be subject to the approval of the City Engineer from the City of Irwindale Public Works Department.

8.3 Storm Water Drainage

Prior to use of the Development Area property as a quarry, stormwater flowed across the site from the north and east to the southwest and would leave the site at its southwest corner and discharge to an existing unimproved drainage basin located on the western portion of the Specific Plan within an SCE easement area. As shown on Figures 8-5 and 8-6, Conceptual Storm Water Management Plan the Specific Plan's storm water management system will mimic the property's historical drainage pattern. Storm water flows will be conveyed across the Development Area via a backbone storm drain network to a detention/infiltration basin, which will be constructed pursuant to the approved LID and will replace the existing unimproved drainage basin. A storm drain lift station will convey water southeast through a 12" private force main and into a proposed 24" public storm drain within Live Oak Avenue, and then into the San Gabriel River.

Low Impact Development (LID) site design strategies and Best Management Practice (BMP) control measures promote the use of natural infiltration (where permitted), evaporation, and use of stormwater. LID strategies include, in order of priority: 1) bioretention/infiltration systems, 2) capture and reuse systems, 3) high efficiency biofiltration systems, and 4) proprietary filtration systems. The primary LID measure for the Development Area includes detention basins, capable of retaining the required water quality volumes, designed with either soft bottoms and/or dry wells for infiltration purposes as water quality measures. Where feasible, subsurface storage chambers that serve as water quality infiltration measures can be implemented within the Development Area. The type and extent of the water quality infiltration measures will ultimately be determined based on geotechnical report findings and recommendations for the Development Area. If infiltration is restricted or not feasible in some areas, based on the geotechnical report findings, then capture and reuse, bio-filtration and/or inlet filters as water quality measures are allowed as treatment control BMPs within the Development Area. All LID and BMP features shall comply with the City of Irwindale Building Code and will require grading and drainage permits from the Building & Safety Division.

8.4 Dry Utilities

As shown on Figures 8-7 and 8-8, Dry Utilities, the Development Area will connect to existing dry utilities (electric, gas, and communication systems) installed within Live Oak Avenue. All dry utilities internal to the Development Area will be installed underground in utility trenches. The locations of trenches, lateral connections, transformers, switches, pull boxes, and dry utility manholes will be determined at the time Buildings are positioned in conjunction with implementing development.

8.5 Solid Waste Disposal

The City currently contracts with Athens Services to provide businesses with a full spectrum of solid waste disposal that includes routine trash removal, recyclable collection, organic waste collection, bulky item removal (upon request only), and hazardous waste removal. And a restaurant food waste composting pilot program. Accordingly, all refuse collection services will be available to the development from Athens Services. The development shall provide trash enclosures to accommodate the size, type, and number of bins required for the uses on site.

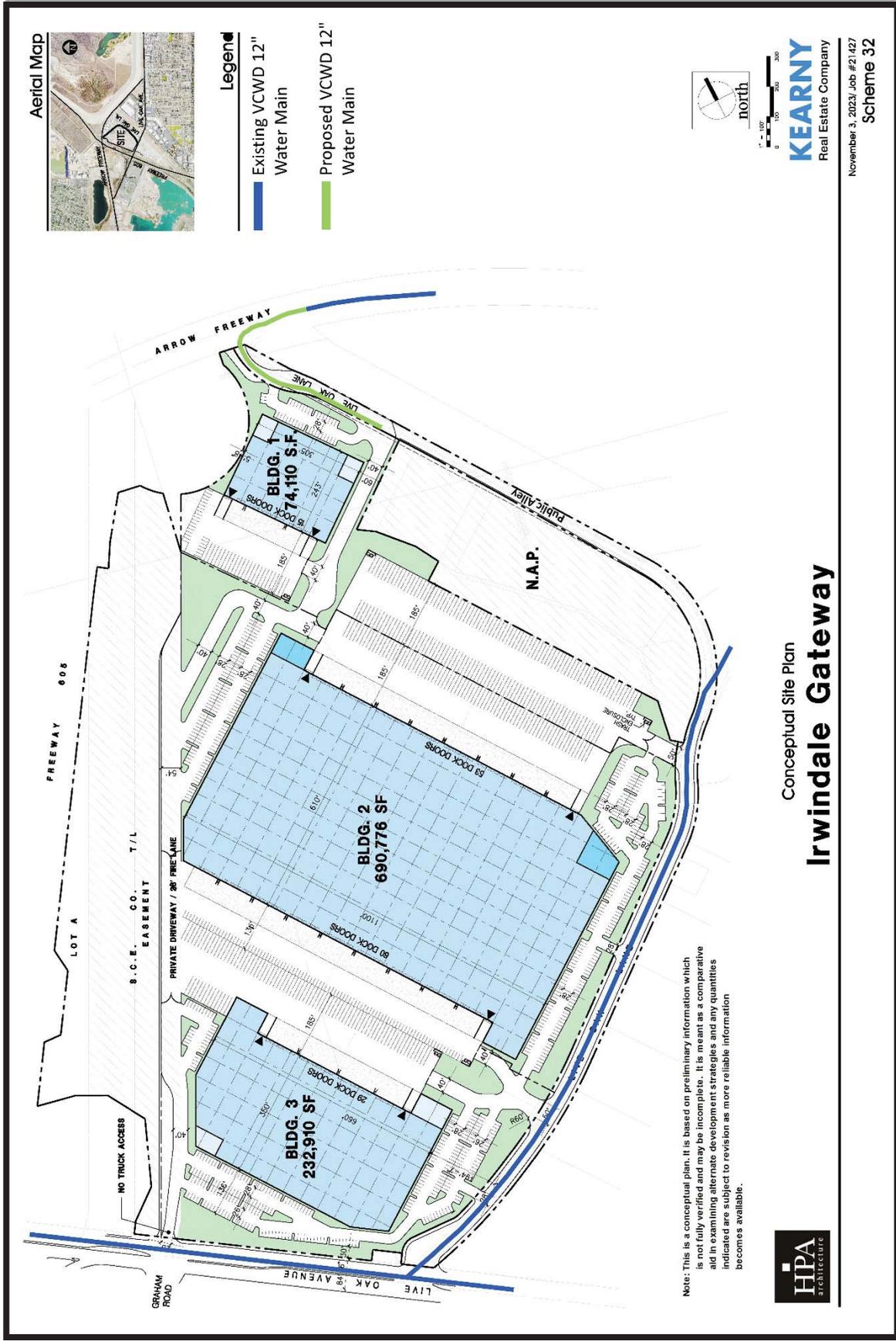


FIGURE 8-1 – CONCEPTUAL WATER PLAN

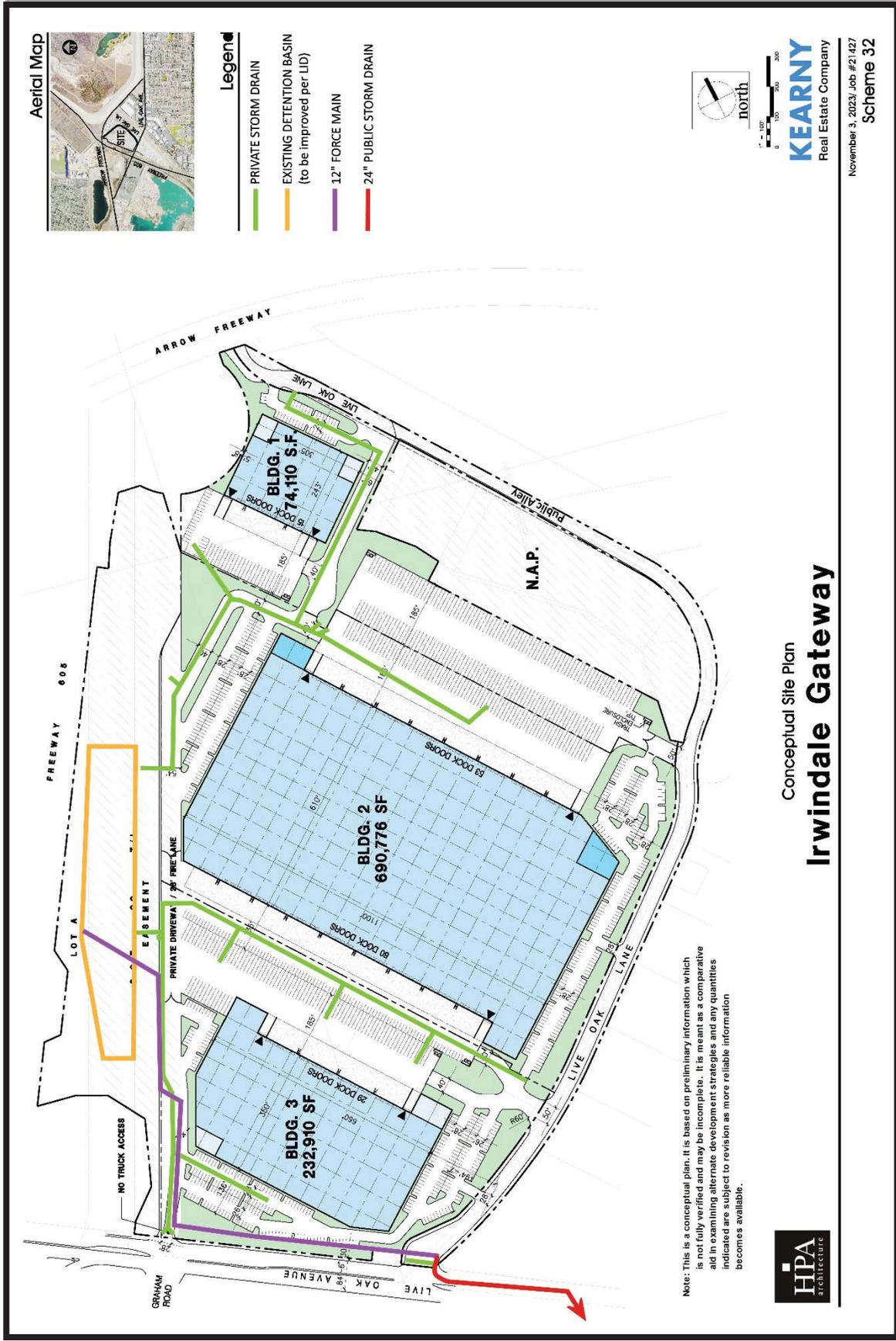


FIGURE 8-3 – CONCEPTUAL STORM WATER MANAGEMENT PLAN

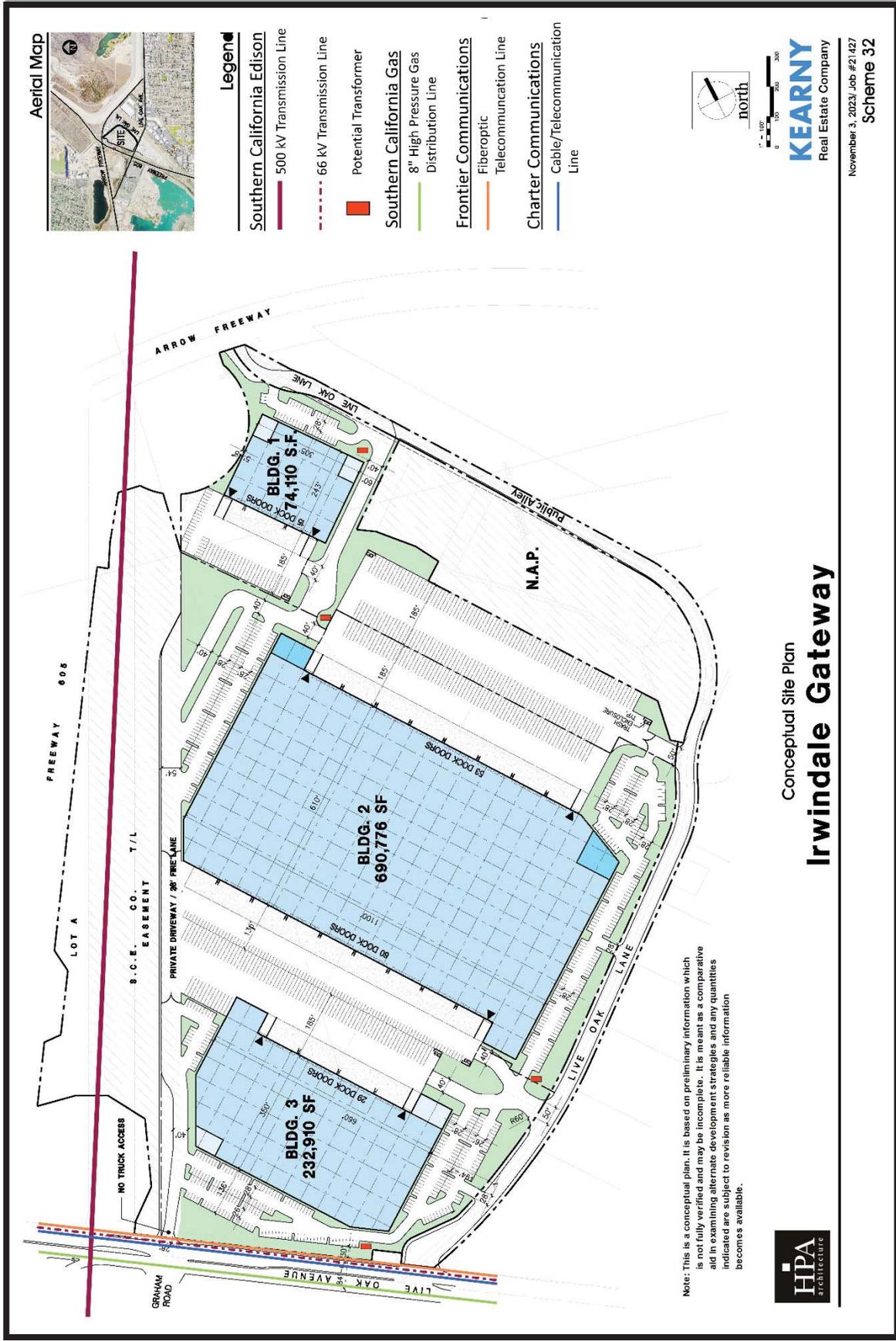


FIGURE 8-4 – CONCEPTUAL DRY UTILITIES PLAN

IMPLEMENTATION

IX.

9. Implementation

9.1 Overview

This section establishes the implementation and review process required for development proposed within the Specific Plan Area. This section provides general administrative provisions; review and approval procedures; and implementation measures, including short-term and ongoing tasks. The provisions contained in the Specific Plan constitute the primary land use and development standards for the Development Area. While the entire Specific Plan constitutes the zoning for the Specific Plan area, Section 6, Development Standards, contains specific zoning regulations for the Development Area. Upon adoption of the Specific Plan by the City of Irwindale, all on- and off-site improvements shall be consistent with the development standards and design guidelines set forth in Sections 6, 7, and 8 of this Specific Plan.

Implementation of development within the Development Area shall be subject to City approval of lot line adjustments, site plan review, plot plans, subdivision reviews, building permits, and other planning approvals and permits that may be required by the City of Irwindale pursuant to the Specific Plan or the City of Irwindale Municipal Code. The implementation process described herein provides the procedures for review and approval of development within the Specific Plan Area.

9.2 Severability

If any portion of this Specific Plan is declared to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions hereof. The legislative body hereby declares that they would have enacted these regulations and each portion thereof irrespective of the fact that any one or more portions be declared invalid or ineffective.

9.3 Interpretations and Determinations

Requests for interpretations of this Specific Plan and verifications relating to prior approvals or permits may be made to the Community Development Director. Requests shall be in writing. The decision of the Community Development Director on such requests may be appealed pursuant to Chapter 17.25 (Common Procedures) of the IMC.

9.4 Site Plan and Design Review

All development within the Development Area shall be subject to Site Plan and Design Review, as set in the Chapter 17.27 (Site Plan and Design Review) of the IMC.

9.5 Conditional Use Permits

Conditional Use Permits are provided for the individual review of uses at specific locations, as outlined in Table 6-1 to ensure that their operation will be compatible with surrounding areas and uses. Applications for Conditional Use Permits shall be reviewed pursuant to Chapter 17.28 (Use Permits) of the IMC.

9.6 Minor Variance

As part of a Site Plan and Design Review application, the Community Development Director or Planning Commission, as applicable, may grant a minor variance to the development standards listed in the Specific Plan, pursuant to Chapter 17.31 (Minor Variances) of the IMC.

9.7 Amendments to the Specific Plan

All modifications to the Specific Plan that do not meet the criteria of a minor modification (see Section 9.6) or any applicable interpretation made pursuant to Section 9.3 shall require an amendment to the Specific Plan Amendments shall be processed in accordance with § 65453, et. seq. of the California Government Code, which requires Specific Plan Amendments be reviewed and adopted in the same manner as a General Plan and Chapter 17.35 of the IMC.

9.8 Subdivision

If a proposed project requires a subdivision, the subdivision shall be consistent with and serve to implement the policies and provisions of the Specific Plan and all applicable City policies and ordinances as required by Title 16 (Subdivisions) of the IMC. All subdivisions shall be authorized through the approval of a map or other approval in compliance with Title 16 (Subdivisions) of the IMC and the California Subdivision Map Act for Land.

9.9 Financing of Development in Development Area

Development in the Specific Plan Area and any off-site improvements to roadways or infrastructure required as conditions of approval or mitigation measures identified as part of the Environmental Impact Report prepared for the project pursuant to CEQA and adopted by the City shall be privately funded. All development shall be subject to Development Impact Fees and any other applicable fees.

9.10 Maintenance Plan

The public and private improvements constructed within the Specific Plan Area shall be maintained through a combination of public and private entities as described in Table 9-2.

Major infrastructure costs may be offset by public assistance such as a Community Facility District (CFD) or other special districts to provide funding for the construction of a variety of public facilities and the provision of public services. City Council approval is a prerequisite for the implementation of all special district-financing mechanisms.

For common areas located within the Development Area, a Private Maintenance Association(s) shall be established with recorded covenants, conditions and restrictions to govern allocation of maintenance responsibilities among property owners. Maintenance of public facilities shall be the responsibility of the appropriate public agency.

TABLE 9-1 | MAINTENANCE RESPONSIBILITIES

FACILITY	PRIVATE MAINTENANCE ASSOCIATION	CITY OF IRWINDALE	PUBLIC UTILITY	OTHER MAINTENANCE ENTITY ¹
CIRCULATION & RELATED FACILITIES				
Live Oak Avenue				
Pavement & Curbs		X		
Landscaping within public right-of-way, including medians and parkways		X		
Sidewalks		X		
Pavement & Curbs		X		
Landscaping within public right-of-way, including medians and parkways		X		
Sidewalks		X		
Live Oak Lane				
Public Alley		X		
Private Drive Aisles	X			
Parking Lots, including landscaping	X			
Traffic Signals ³		X		
Traffic Signs				
Within public right-of-way		X		
Within private property	X			
Street Lights				
Within public right-of-way			X	
Within private property	X			
Truck Traffic Management Plan Directional Signage	X			
LANDSCAPING & RELATED FACILITIES				
Common area landscaping, including entry treatments at Live Oak Avenue and Arrow Highway	X			
Monuments and Signage	X			
Walls and Fences	X			
UTILITY INFRASTRUCTURE				
On-site water facilities/infrastructure	X		X	
On-site sanitary sewer facilities/infrastructure	X	X		
Private storm water drainage facilities/infrastructure	X			
Public storm water drainage facilities/infrastructure		X		X
Dry utilities (electricity, natural gas, communications systems)			X	X

TABLE 9-1 MAINTENANCE RESPONSIBILITIES				
FACILITY	PRIVATE MAINTENANCE ASSOCIATION	CITY OF IRWINDALE	PUBLIC UTILITY	OTHER MAINTENANCE ENTITY ¹
Notes: 1. Other Maintenance Entities may include Caltrans, utility providers, and other public/private entities that may maintain billboards and stormwater drainage facilities on-site. 2. Traffic signals may require maintenance easements for the City to access and maintain the traffic signals.				

9.11 California Environmental Quality Act

An Environmental Impact Report (the “EIR”) prepared in compliance with the requirements of the California Environmental Quality Act (“CEQA”) for the Specific Plan identified environmental impacts associated with the project requiring mitigation.

The EIR identifies potential impacts resulting from development and establishes mitigation measures to reduce the impacts to a less than significant level. As the Lead Agency, the City of Irwindale has adopted a Mitigation Monitoring and Reporting Program (MMRP) as part of this Specific Plan.

The EIR will serve as the primary environmental clearance document for the Specific Plan and all future development undertaken within the Specific Plan Area. The EIR is considered the primary environmental clearance document for the implementation of the project, including infrastructure, roadway, and any other related on-/off-site improvements. Development applications that require discretionary review will be examined in consultation with the EIR to determine if additional environmental documentation is required. No further analysis would be conducted on projects determined to be exempt from CEQA or in full compliance with the adopted EIR. However, the project applicant will be required to submit documentation evidencing said development is allowed and in conformance with the Specific Plan and that the potential environmental effects are within the parameters analyzed within the EIR.

ATTACHMENT “E”

ORDINANCE NO. 788

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF IRWINDALE MODIFYING THE CITY OF IRWINDALE ZONING MAP (ZONE CHANGE NO. 02-2022) FROM M-2 (HEAVY MANUFACTURING) TO IRWINDALE GATEWAY SPECIFIC PLAN FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-047) AND FINDING THE PROJECT CONSISTENT WITH THE CERTIFIED ENVIRONMENTAL IMPACT REPORT (SCH# 2023020290)

WHEREAS, Jeff Dritley, on behalf of KP Irwindale Owner, LLC 1875 Century Park East, Suite 380, Los Angeles, CA 90067, the Applicant, has made the following request: 1) Zone Change (ZC) No. 02-2022 to change the City of Irwindale Zoning Map from M-2 (Heavy Manufacturing) to Irwindale Gateway Specific Plan for property located at 13620 Live Oak Lane – APNs: 8532-002-046 and 8532-002-047. (This ZC is being processed concurrently with FEIR State Clearinghouse No. 2023020290, GPA No. 02-2022, ZOA No. 02-2023, and TPM No. 83854); and

WHEREAS, Section 17.84.010 of the Irwindale Municipal Code allows for changes to the zoning and the classification of property “whenever the public interest and necessity so require”; and

WHEREAS, the proposed Irwindale Gateway Specific Plan, provides for the planned use and long-term development of the business park for the next several years; and

WHEREAS, the City Council, in connection with the implementing actions necessary for the adoption of the Irwindale Gateway Specific Plan, has considered the proposed Zone Change No. 02-2022, an amendment to the Zoning Code and the Zoning Map designation for two (2) parcels (APNs: 8532-002-046 and 8532-002-047) to the Irwindale Gateway Specific Plan; and

WHEREAS, pursuant to the authority and criteria contained in the California Environmental Quality Act (CEQA) of 1970, as amended, and the City of Irwindale environmental guidelines, the City, as the Lead Agency, has analyzed the project and has prepared an Environmental Impact Report (“EIR”). The EIR was circulated for public review for 45 days from May 13, 2024 through June 27, 2024. A copy of the EIR was circulated through the State Clearinghouse (SCH# 2023020290), posted on the City’s website, and was available at the Irwindale Public Library, City Clerk’s Office, and Community Development Department. A copy of the EIR and Mitigation Monitoring and Reporting Program was posted on the project’s dedicated website <https://www.irwindaleca.gov/590/13620-Live-Oak-Lane---Irwindale-Gateway->; and

WHEREAS, the public review period for the Draft EIR ended on June 27, 2024; and

WHEREAS, a Final Environmental Impact Report (SCH# 2023020290) was prepared for the proposed project, including the Draft EIR, comments received on the Draft EIR and responses to those comments, and revisions and corrections to the Draft EIR made in response to comments received; and

WHEREAS, On October 28, 2024, the Irwindale Planning Commission conducted a duly noticed public hearing, as required by law, on the Application, closed the public hearing and requested that the item be continued until the entire Planning Commission was present. The item was continued the meeting to a date certain (November 26, 2024); and

WHEREAS, the Special Planning Commission meeting of November 26, 2024 was later rescheduled to November 14, 2024; and

WHEREAS, revised notices of public hearing were published, posted, and mailed in accordance with the laws, including CEQA, Government Code, and policies of the City of Irwindale; and

WHEREAS, on November 14, 2024, the Planning Commission conducted a duly noticed public hearing on the Irwindale Gateway Specific Plan and corresponding Zone Change No. 02-2022, and adopted PC Resolution No. 837(24) recommending that the City Council of the City of Irwindale adopt Zone Change No. 02-2022 to modify the City of Irwindale Zoning Map from M-2 (Heavy Manufacturing) to Irwindale Gateway Specific Plan; and

WHEREAS, notice of a public hearing before the City Council on the proposed Zone Change for the Specific Plan project, was given in accordance with applicable law; and

WHEREAS, on January 8, 2025, the Irwindale Planning Commission continued its hearing on this item to a date certain (January 22, 2025); and

WHEREAS, on January 22, 2025, the City Council conducted a duly noticed public hearing, as required by law, on the proposed Application at which time oral and documentary evidence was introduced along with the written recommendation of the City of Irwindale City Council, received public testimony, and directed Staff to revise the draft Specific Plan document, Resolutions and Ordinances to remove all references to Battery Energy Storage Systems (BESS), and put on the February 26, 2025 Consent Calendar; and

WHEREAS, on February 26, 2025, the City Council adopted the revised Resolutions, Ordinances and Specific Plan document; and

WHEREAS, all legal prerequisites to the adoption of this Ordinance have occurred.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF IRWINDALE DOES ORDAIN AS FOLLOWS:

SECTION 1. The City Council hereby specifically finds that all of the facts set forth in Recitals, Part A, of this Ordinance are true and correct.

SECTION 2. Based upon substantial evidence presented to this City Council during the public hearing conducted with regard to the Application, including written staff reports, verbal testimony, and evidence in the City's record for the project, this City Council hereby specifically finds as follows:

- a. The proposed Zone Change from M-2 [Heavy Manufacturing) to Irwindale Gateway Specific Plan is intended to encourage the highest and best use for the subject property and is in the best interests and welfare of the City and its residents.
- b. The proposed Zone Change is internally consistent with all other provisions of the General Plan. The Community Development Element of the General Plan promotes several policies that support the adoption of the SP and associated entitlements. Community Development Element Policy 1 strives to preserve the overall mix of land uses and development in the community. Community Development Element Policy 3 seeks to ensure that the type, location, and intensity of all new development and intensified developments adhere to the requirements specified in its particular land use category. Community Development Element Policy 5 promotes the comprehensive development consistent with the General Plan as opposed to piecemeal and incremental planning. If adopted, the proposed Zone Change would implement the proposed Irwindale Gateway Specific Plan, which implements policies set forth in the General Plan, to provide direction for the development of the business park over several years, addresses sustainable practices on and off-site, promotes compatibility with surrounding areas, and provides a unique and flexible policy document that facilitates orderly development of the site.
- c. The proposed Zone Change will not be detrimental to the public interest, health safety, convenience, or welfare of the City. The purpose of the proposed Zone Change is to implement the Specific Plan, which provides a comprehensive framework for the long-term growth and development of the entire business park, provides clarity in processes and in the coordination between the applicant, developers, and City. If adopted, the proposed Specific Plan would define permitted uses, regulate the sizes and locations of buildings,

specify design guidelines and development standards, address mobility and connectivity issues, identify project infrastructure and services, and provide administrative and implementation language for the entire 66.64-acre property.

- d. The affected properties are physically suitable in terms of design, location, operating characteristics, shape, size, topography, and the provision of public and emergency vehicle access, and public services and utilities and is served by highways and streets adequate in width and improvement to carry the kind and quantity of traffic the proposed use would likely generate, to ensure that the proposed use(s) and/or development will not endanger, jeopardize, or otherwise constitute a hazard to the property or improvements in the vicinity in which the property is located. The site and adjacent environs were studied for physical suitability as part of the preparation for the Specific Plan and associated Final Environmental Impact Report.

SECTION 3. Pursuant to the authority and criteria contained in the California Environmental Quality Act (CEQA) of 1970, as amended, and the City of Irwindale environmental guidelines, the City Council finds that the Environmental Impact Report (EIR) for The Irwindale Gateway Specific Plan (State Clearinghouse (SCH) No. 2023020290), including the Mitigation Monitoring and Reporting Program and Findings of Fact and Statement of Overriding Considerations adequately describes the Project and its potential impacts, as well as the impacts potentially resulting from the approval of the Irwindale Gateway Specific Plan for the purposes of CEQA review, and it is consistent with the analysis of the Project within the City Council certified Final Environmental Impact Report, adopted Mitigation Monitoring and Reporting Program, adopted Findings of Fact, and adopted Statement of Overriding Considerations.

SECTION 4. Based upon substantial evidence and testimony received at public hearing and the findings and conclusions set forth hereinabove, the City Council adopts and approves proposed Zone Change No. 02-2022 to change the Zoning Map as shown in attached Exhibit "A" to this Ordinance.

SECTION 5. If any section, subsection, subdivision, paragraph, sentence, clause or phrase, or portion of this Ordinance is, for any reason, held to be unconstitutional or invalid or ineffective by any court of competent jurisdiction, such decision shall not affect the validity or effectiveness of the remaining portions of this Ordinance or any part thereof. The City Council hereby declares that it would have adopted this Ordinance and each section, subsection, subdivision, paragraph, sentence, clause or phrase of this Ordinance irrespective of the fact that one or more sections, subsections, subdivisions, paragraphs, sentences, clauses or phrases be declared unconstitutional or invalid or ineffective. To this end, the provisions of this Ordinance are declared severable.

SECTION 6. This Ordinance shall take effect thirty (30) days after its adoption. The City Council hereby authorizes and directs the Mayor and the Chief Deputy City Clerk to execute this Ordinance on behalf of the City of Irwindale forthwith upon its adoption.

SECTION 7. The Chief Deputy City Clerk shall certify as to the passage of this Ordinance and shall cause the same to be published and/or posted at the designated locations in the City of Irwindale

PASSED, APPROVED AND ADOPTED this 26th day of February 2025.

Larry G. Burrola, Mayor

ATTEST:

Laura M. Nieto, MMC
Chief Deputy City Clerk
STATE OF CALIFORNIA }
COUNTY OF LOS ANGELES } ss.
CITY OF IRWINDALE }

I, Laura M. Nieto, Chief Deputy City Clerk of the City of Irwindale, do hereby certify that the foregoing Ordinance No. 788 was duly introduced at a regular meeting of the Irwindale City Council held on the 26th day of February 2025, and was duly approved and adopted on second reading at its regular meeting held on the 12th day of March 2025, by the following vote of the Council:

AYES: Councilmembers:

NOES: Councilmembers:

ABSENT: Councilmembers:

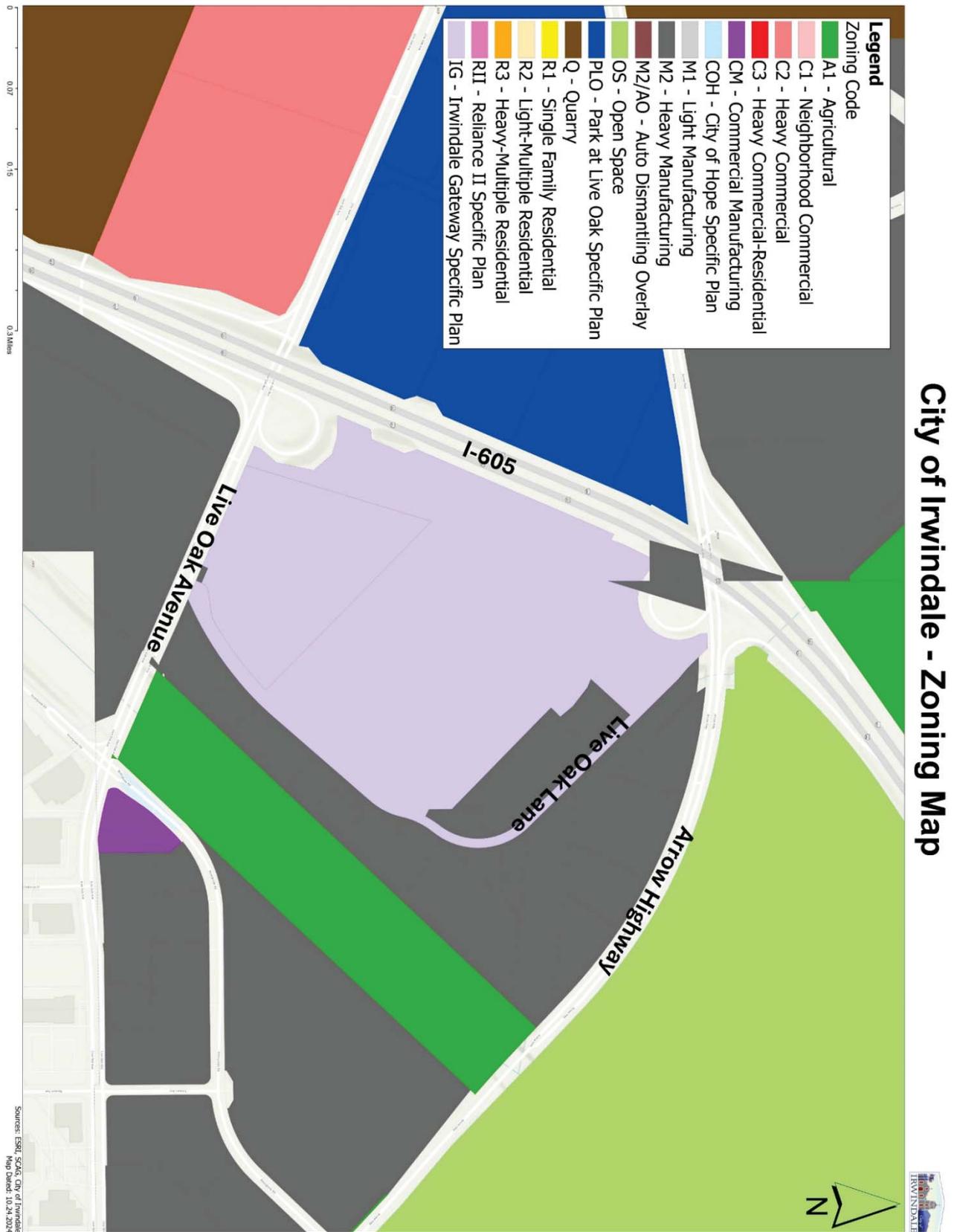
ABSTAIN: Councilmembers:

Laura M. Nieto, MMC
Chief Deputy City Clerk

Exhibit:

A. Proposed Zoning Map

EXHIBIT "A"



ATTACHMENT “F”

RESOLUTION NO. 2025-05-3593

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IRWINDALE APPROVING TENTATIVE PARCEL MAP NO. 83854 TO CREATE SEVEN (7) PARCELS FOR PROPERTY LOCATED AT 13620 LIVE OAK LANE, IRWINDALE, CA (APNS: 8532-002-046 AND 8532-002-04) IN THE M-2 (HEAVY MANUFACTURING) ZONE SUBJECT TO CONDITIONS AS SET FORTH HEREIN, MAKING FINDINGS IN SUPPORT THEREOF

A. RECITALS.

- (i) Jeff Dritley, on behalf of KP Irwindale Owner, LLC 1875 Century Park East, Suite 380, Los Angeles, CA 90067, the Applicant, has made the following request: 1) Tentative Parcel Map (TPM) No. 83854 to create seven (7) parcels for property located at 13620 Live Oak Lane – APNs: 8532-002-046 and 8532-002-047. (This TPM is being processed concurrently with FEIR State Clearinghouse No. 2023020290, GPA No. 02-2022, ZOA No. 02-2023, and ZC No. 02-2022); and
- (ii) The proposed application will allow for the creation of seven (7) individual parcels for future industrial development in accordance with the proposed Specific Plan and the Subdivision Map Act.
- (iii) Pursuant to the authority and criteria contained in the California Environmental Quality Act (CEQA) of 1970, as amended, and the City of Irwindale environmental guidelines, the City, as the Lead Agency, has analyzed the project and has prepared an Environmental Impact Report (“EIR”). The EIR was circulated for public review for 45 days from May 13, 2024 through June 27, 2024. A copy of the EIR was circulated through the State Clearinghouse (SCH# 2023020290), posted on the City’s website, and was available at the Irwindale Public Library, City Clerk’s Office, and Community Development Department. A copy of the EIR and Mitigation Monitoring and Reporting Program was posted on the project’s dedicated website <https://www.irwindaleca.gov/590/13620-Live-Oak-Lane---Irwindale-Gateway->; and
- (iv) The public review period for the Draft EIR ended on June 27, 2024; and
- (v) A Final Environmental Impact Report (SCH# 2023020290) was prepared for the proposed project, including the Draft EIR, comments received on the Draft EIR and responses to those comments, and revisions and corrections to the Draft EIR made in response to comments received; and

- (vi) On October 28, 2024, the Irwindale Planning Commission continued the hearing on this item to a date certain (November 26, 2024); and
- (vii) The Special Planning Commission meeting of November 26, 2024 was later rescheduled to November 14, 2024; and
- (viii) Revised notices of public hearing were published, posted, and mailed in accordance with the laws, including CEQA, the Government Code, and policies of the City of Irwindale; and
- (ix) On November 14, 2024, the Planning Commission conducted a duly noticed Special public hearing on the Irwindale Gateway Specific Plan and corresponding Zone Ordinance Amendment No. 02-2023, at which time, the Planning Commission opened the public hearing, took testimony on the Application, at which time they received input from staff, the Assistant City Attorney, and the Applicant, heard public testimony, discussed the Proposed Project; and closed the public hearing; and
- (x) On December 16, 2024, notice of a public hearing before the City Council on the proposed project adoption, was given in accordance with applicable law; and
- (xi) On January 8, 2025, the City Council continued the hearing on this item to a date certain (January 22, 2025); and
- (xii) On January 22, 2025, the City Council conducted a duly noticed public hearing, as required by law, on the proposed Application at which time oral and documentary evidence was introduced along with the written recommendation of the City of Irwindale City Council, received public testimony, and directed Staff to revise the draft Specific Plan document, Resolutions and Ordinances to remove all references to Battery Energy Storage Systems (BESS), and put on the February 26, 2025 Consent Calendar; and
- (xiii) On February 26, 2025, the City Council adopted the revised Resolutions, Ordinances and Specific Plan document; and
- (xiv) All legal prerequisites to the adoption of this Resolution have occurred.

B. RESOLUTION.

NOW, THEREFORE, it is hereby found, determined and resolved by the City Council of the City of Irwindale as follows:

1. The City Council hereby specifically finds that all of the facts set forth in Recitals, Part A, of this Resolution are true and correct and are incorporated herein by this reference.

2. Based upon substantial evidence presented to this City Council during the public hearing conducted with regard to the Application, including written staff reports, verbal testimony, site plans and Conditions of Approval attached hereto as Exhibit "A," this City Council hereby specifically finds as follows:

Tentative Parcel Map 83854

- a. *The proposed map is consistent with the General Plan, applicable specific plans, and the Zoning Code.*

The proposed map is for the subdivision of a property for development of industrial and commercial uses. The subject property has a General Plan land use designation of Regional Commercial and is zoned M-2 (Heavy Manufacturing). The proposed map is being processed concurrently with General Plan Amendment (GPA) No. 02-2022 to amend the Community Development Element and Land Use Plan Map to designate the property as Irwindale Gateway Specific Plan, ZOA 02-2023 to adopt the Irwindale Gateway Specific Plan, and ZC 02-2022 to change the Zoning Map designation from M-2 (Heavy Manufacturing) to Irwindale Gateway Specific Plan. The Specific Plan proposes an industrial business park to include development standards concerning a) facilities and improvements; b) pedestrian and vehicular ingress, egress, and internal circulation; c) setbacks; d) height of buildings; e) mechanical and utility service equipment; f) landscaping; g) grading; h) lighting; i) parking; j) drainage; and k) intensity of land use so as to avoid traffic congestion, to ensure the public health, safety, and welfare, and to prevent adverse effects on surrounding properties. The proposed project involves redevelopment of a site that is a former landfill, and sand and gravel quarry. Therefore, the proposed map is consistent with the General Plan, as amended, the Irwindale Gateway Specific Plan, and the Zoning Code.

- b. *The design or improvement of the proposed subdivision is consistent with the General Plan, applicable specific plans, and the Zoning Code.*

The proposed project is for development of an industrial business park. The proposed map is being processed concurrently with General Plan Amendment (GPA) No. 02-2022 to amend the Community Development Element and Land Use Plan Map to designate the property as Irwindale Gateway Specific Plan, ZOA 02-2023 to adopt the Irwindale Gateway Specific Plan, and ZC 02-2022 to change the Zoning Map designation from M-2 (Heavy Manufacturing) to Irwindale Gateway Specific Plan. The Specific Plan proposes an industrial business park to include development. The design and improvement of the proposed subdivision is consistent with development standards established in

the Specific Plan document with respect to land use, building height, parking, and landscaping.

c. The site is physically suitable for the type of development proposed.

The 66.64-acre site is suitable for the proposed Specific Plan industrial business park development, which provides for a maximum of 997,796 feet of building space, with private driveways and drive aisles, landscaping, and parking areas. The site is ±66.64-acres site currently served by an existing private street, Live Oak Lane. Live Oak Lane in its entirety, is a part of the subject property. As part of the improvements, a total of 2,160 feet of Live Oak Lane will be dedicated to the City and improved to the City's standard of 60'-0". The remaining areas are already paved and will be converted to a public alley to connect the northern and southern sections of Live Oak Lane. In addition to the improved roadways, there will also be new sidewalk and landscaped setbacks.

d. The site is physically suitable for the proposed density or intensity of development.

The 66.64-acre site is likewise suitable for the proposed intensity of the Specific Plan industrial business park, which provides for a maximum of 997,796 feet of building space, with private driveways and drive aisles, landscaping, and parking areas. The site is ±66.64-acres site currently served by an existing private street, Live Oak Lane. Live Oak Lane in its entirety, is a part of the subject property. As part of the improvements, a total of 2,160 feet of Live Oak Lane will be dedicated to the City and improved to the City's standard of 60'-0". The remaining areas are already paved and will be converted to a public alley to connect the northern and southern sections of Live Oak Lane. In addition to the improved roadways, there will also be new sidewalk and landscaped setbacks.

e. The design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage nor substantially and avoidably injure fish or wildlife or their habitat.

The design or proposed improvements of the subdivision will not cause any substantial environmental damage or substantially injure fish, wildlife, or their habitats, or cause serious public health problems in that the Application will create seven (7) parcels from two (2) parcels in an area that has been planned for industrial and commercial uses. The subdivision is located on a former landfill and sand and gravel quarry within an urbanized area and is not the habitat of fish or wildlife. Pursuant to the authority and criteria contained in the California Environmental Quality Act (CEQA) of 1970, as amended, and the City of Irwindale environmental guidelines, the City, as the Lead Agency, has analyzed the project and has prepared an Environmental Impact Report

("EIR"), for The Irwindale Gateway Specific Plan (State Clearinghouse (SCH) No. 2023020290), including the Mitigation Monitoring and Reporting Program and Findings of Fact and Statement of Overriding Considerations which adequately describes the Project and its potential impacts, as well as the impacts potentially resulting from the approval of the Irwindale Gateway Specific Plan for the purposes of CEQA review, and the proposed Tentative Parcel Map is consistent with the analysis of the Project.

- f. The design of the subdivision or type of improvements will not pose a threat to the public health, safety, and welfare.*

The design of the subdivision and improvements will not pose a threat to the public health, safety, and welfare. Adequate fire and police protection is available to serve the project. Uses permitted and conditionally permitted within the proposed project are required to meet all Federal, State and local health and safety codes and regulations.

- g. The design of the subdivision or the type of improvements will not conflict with easements acquired by the public at large for access through or use of property within the proposed subdivision.*

There is an existing, 9.61-acre Southern California Edison (SCE) easement on the western portion of the site. Part of the Specific Plan's infrastructure plan includes the development of a new detention basin within this easement for the collection of project storm water flows. However, this will not conflict with easements acquired by the public at large for access through or use of the property within the proposed subdivision.

- h. Any discharge of waste from the proposed subdivision into an existing sewer system would not result in violation of existing requirements prescribed by the California Regional Water Quality Control Board.*

The proposed subdivision is served by an existing City sewer system and is required to comply with all requirements of the California Regional Water Quality Control Board.

- i. The requirements of CEQA have been satisfied.*

Pursuant to Pursuant to the authority and criteria contained in the California Environmental Quality Act (CEQA) of 1970, as amended, and the City of Irwindale environmental guidelines, the City, as the Lead Agency, has analyzed the project and has prepared an Environmental Impact Report ("EIR"), SCH# 2023020290. Even after the adoption of all feasible Mitigation Measures as identified in the Mitigation Monitoring and Reporting Program (MMRP), certain significant or potentially significant environmental effects caused by the Project

directly, or cumulatively, will remain and a Statement of Overriding Considerations is recommended to be adopted as part of the project.

3. Pursuant to the authority and criteria contained in the California Environmental Quality Act (CEQA) of 1970, as amended, and the City of Irwindale environmental guidelines, the City, as the Lead Agency, has analyzed the project and has prepared an Environmental Impact Report (“EIR”). The EIR was circulated for public review for 45 days from May 13, 2024 through June 27, 2024. A copy of the EIR was circulated through the State Clearinghouse (SCH# 2023020290), posted on the City’s website, and was available at the Irwindale Public Library, City Clerk’s Office, and Community Development Department. A copy of the EIR and Mitigation Monitoring and Reporting Program was posted on the project’s dedicated website <https://www.irwindaleca.gov/590/13620-Live-Oak-Lane---Irwindale-Gateway->. The EIR for The Irwindale Gateway Specific Plan (State Clearinghouse (SCH) No. 2023020290), including the Mitigation Monitoring and Reporting Program and Findings of Fact and Statement of Overriding Considerations adequately describe the Project and its potential impacts, as well as the impacts potentially resulting from the approval of the Irwindale Gateway Specific Plan for the purposes of CEQA review, and the proposed Tentative Parcel Map is consistent with the analysis of the Project.

4. Based upon the substantial evidence and conclusions set forth herein above, this City Council hereby approves the Application subject to the conditions set forth in Exhibit “A” attached hereto and by this reference incorporated herein, which conditions are deemed necessary to protect the public health, safety and general welfare and are reasonable and proper in accordance with the intent and purposes of Title 16 and Title 17 of the Irwindale Municipal Code.

5. The Secretary shall:

- a. Certify to the adoption of this Resolution; and
- b. Forthwith transmit a certified copy of this Resolution, by mail, to the Applicant at the address of record set forth in the Application.

PASSED, APPROVED AND ADOPTED this 26th day of February 2025.

Larry G. Burrola, Mayor

ATTEST:

Laura M. Nieto, MMC
Chief Deputy City Clerk
STATE OF CALIFORNIA }

COUNTY OF LOS ANGELES } ss.
CITY OF IRWINDALE }

I, Laura M. Nieto, Chief Deputy City Clerk of the City of Irwindale, do hereby certify that the foregoing Resolution No. 2025-05-3593 was adopted at a regular meeting of the Irwindale City Council held on the 26th day of February 2025, by the following vote of the Council:

AYES: Councilmembers:

NOES: Councilmembers:

ABSENT: Councilmembers:

ABSTAIN: Councilmembers:

Laura M. Nieto, MMC
Chief Deputy City Clerk

Exhibits:

A: Conditions of Approval

EXHIBIT “A”

CITY COUNCIL RESOLUTION NO. 2025-05-3593

**Tentative Parcel Map No. 83854
KP Irwindale Owner, LLC
13620 Live Oak Lane – The Irwindale Gateway Specific Plan**

A. GENERAL

1. Tentative Parcel Map No. 83854, a true and correct copy attached hereto, allows for the subdivision of the property, which will result in the creation seven (7) lettered parcels as described on the plans dated **November 26, 2024**.
2. The Applicant shall defend, indemnify and hold harmless the City of Irwindale, its agents, officers, or employees from any claims, damages, action, or proceeding against the City or its agents, officers, or employees to attack, set aside, void or annul, any approval of the City, its advisory agencies, appeal boards, or legislative body concerning Tentative Parcel Map 83854. The City will promptly notify the permittee of any such claim, action, or proceeding against the City and will cooperate fully in the defense.
3. The use and improvements authorized by this Tentative Parcel Map shall conform to the plans as finally approved by the City as conditioned herein, and any appreciable modification of the plans or mode of operation, as determined by the City Engineer/Building Official and Community Development Manager/City Planner, shall require the prior approval of the City Council pursuant to the amendment of the Tentative Parcel Map.
4. The Applicant agrees to allow City inspectors access to the site to reasonably inspect the site during normal working hours to assure compliance with these conditions and other codes. Any and all fees required to be paid to any public agency shall be paid prior to obtaining any permits for this project.
5. The Applicant shall maintain and use the project location and facility thereon in full compliance with all codes, standards, policies and regulations imposed by the City, County, State, or Federal agencies with jurisdiction over the facility.
6. It shall be required that the subject location and its contents, including but not limited to, structures, fences or garden/block walls, and vehicles are maintained free and clear of any graffiti. The Applicant shall be held responsible for the immediate removal of any and all graffiti found on-site within 48 hours of its application.

7. To the extent that lighting is governed by the Specific Plan, any inconsistencies shall be subject to the requirements of the IMC. Illumination intensity during hours of darkness shall be as follows:
 - To illuminate parking lots or parking structures and their pedestrian links that provide more than five parking spaces for use by the general public, shall provide a minimum coverage of one foot-candle of light with a maximum of eight foot-candles on the parking or walkway surface, unless otherwise approved by the City of Irwindale for visibility and security.
 - To illuminate aisles and passageways within a Building complex, provide a maximum of one-half to one foot-candle of maintained lighting.
8. The premises will be secured with appropriate security lighting, and shall comply with the Specific Plan. A photometric lighting plan shall be submitted, subject to the review and approval of the Community Development Department and the Police Department.
9. In accordance with the provisions of Government Code Section 66020(d)(1), the imposition of fees, dedications, reservations, or exactions for this project are subject to protest by the applicant at the time of approval or conditional approval of the project, or within 90 days after the date of imposition of the fees, dedications, reservations, or exactions imposed on the project.
10. Property owner and/or their designee shall require tenants to provide information regarding their employees' transportation to and from work, within one (1) year but no earlier than nine (9) months of the issuance of the tenant's Occupancy Permit or when building occupancy reaches 85%, whichever occurs first. Each property tenant shall be required to provide the information to the Public Works/Engineering and Community Development Departments. Employee data may be collected by a survey, employee interview or other selected method and shall include the following information: 1) transportation mode(s) used to commute to work, 2) number of days mode used per week, 3) distance of round trip, and 4) city of origin. The results of the data collection shall be tabulated in a clear format. The results shall be submitted to the Public Works/Engineering and Community Development Departments within thirty (30) days of data tabulation. Non-compliance may be subject to enforcement action, including but not limited to penalties.

B. COMMUNITY DEVELOPMENT DEPARTMENT

1. The following conditions shall apply unless the item is addressed in the Specific Plan; in which case the Specific Plan shall govern.

2. All landscaping for the project shall be drought resistant low water with drip irrigation, low flow bubblers and water efficient rotor heads where applicable. Native plants shall be used where feasible.
3. Landscape and irrigation plans shall be prepared by a licensed landscape architect, and are subject to the approval of the Community Development Director and the City Engineer. Landscape plans shall be consistent with the Commercial and Industrial Design Guidelines. Landscape plans shall be consistent with the Specific Plan. Vision clearance shall be maintained at all vehicle entrances and exits. Landscape planters throughout the site shall incorporate river rock.
4. Project landscaping shall comply with Irwindale Municipal Code Chapter 15.30, "Water Efficient Landscape Standards and Guidelines."
5. A complete, permanent, automatic irrigation system shall be provided for all landscaped areas.
6. All landscaped planters shall be surrounded by a six (6) inch horizontal concrete curb.
7. The following invasive plants shall not be used in landscaping:
 - *Carpobrotus edulis* (ice plant)
 - *Hedera helix*, *H. Hibernica*, *H. canariensis* (English ivy, Irish ivy, Algerian ivy)
 - *Vinca Major* (periwinkle)
 - *Pennisetum setaceum* and all cultivars and varieties (fountain grass)
 - *Cortaderia selloana*, *C. jubata* and all cultivars and varieties (pampas grass)
 - *Retama monosperma*, *Genista monspessulana*, *Cytisus striatus*, *Cytisus scoparius*, and *Spartium junceum* (broom – bridal, French, Portuguese, Scotch, Spanish)
 - *Acacia Cyclops* (acacia or western coastal wattle)
 - *Myoporum laetum* (myoporum)
 - *Washingtonia robusta* and *Phoenix canariensis* (Mexican fan palm and Canary Island date palm)
 - *Schinus terevinthifolius* (Brazilian pepper)
 - *Eucalyptus globules*, *E. camaldulensis* (eucalyptus, blue gum, and red gum)
8. All plant material, including trees, shall be maintained in good condition and replaced in the event they die or become diseased.
9. All perimeter fencing, block walls, etc. shall be maintained in satisfactory condition in accordance with all applicable codes.
10. All utility equipment such as backflow units and transformers shall be screened with evergreen screen shrubs as allowed.

11. Any masonry walls and gates shall be decorative consistent with the design criteria set forth in the Specific Plan. The design of the walls and gates shall be subject to the review and approval of the Community Development Department.
12. All building design and construction, including the type, texture, color, and durability of the exterior building materials shall comply with the design criteria set forth in the Specific Plan.
13. Trash enclosures (6'-0" high min.) with solid metal self-closing and self-latching gates shall be provided. The enclosures shall be covered and built with decorative materials to match the type, texture, and color of the materials used in the construction of the buildings. Gates shall remain closed at all times when the trash receptacles are not in use.
14. All rooftop mechanical equipment, including heating and air conditioning units, antennas, and other electronic devices, shall be completely and decoratively screened from view from all public rights of way and adjacent properties and shall be integrated into the design and construction of the buildings¹. All rooftop equipment and screening shall be shown on the plans and elevations, and shall be consistent with the building design and construction materials in texture and color. Such rooftop equipment screening shall be subject to the review and approval of the Community Development Department.
15. The street numbers for the development shall be painted on the rooftop of each building in such a manner that it is clearly visible to public safety personnel and shall be a minimum five (5) feet in length painted with minimum one (1) foot wide brush strokes. Rooftop numbers shall be shown on the plans submitted for plan check.
16. A lighting plan shall be submitted for approval by the Community Development Manager/City Planner describing lighting fixtures for parking lot and building exterior lighting. Lighting fixtures shall be designed to shield light and/or direct light in a downward direction to minimize light spillover to adjacent residential areas.
17. Applicant shall obtain approval from the Community Development Director or their designee for a comprehensive sign program for all project signs prior to construction or installation of any signs on the project site.
18. The Applicant shall comply with the provisions of Irwindale Municipal Code Chapter 17.66, "Trip Reduction and Travel Demand Measures." All required trip reduction and travel demand measures applicable to the project pursuant to Chapter 17.66 shall be indicated on plans at the time of plan check.

¹ Photovoltaic equipment is exempt from this requirement.

19. A chain link fence with green screening shall be installed and maintained around the perimeter of the site at all times during construction.
20. Applicant shall at all times comply with the Irwindale Municipal Code Noise Standards (as may be amended) as measured at the Site boundary. Additionally, if noise impacts exceed the applicable noise standard contained in the Irwindale Municipal Code, Applicant shall take necessary actions and implement procedures to bring the operations into compliance with this Code.
21. To the extent feasible, Applicant shall participate in Southern California Edison's "Savings by Design" program.
22. Development shall adhere to all Mitigation Measures identified in the adopted Mitigation Monitoring and Reporting Program (MMRP), made a part of the Final Environmental Impact Report (SCH# 2023020290) associated with the project in compliance with the California Environmental Quality Act (CEQA).
23. The Applicant shall prepare Covenants, Conditions and Restrictions (CC&Rs) for the development, subject to the review and approval of the City Engineer, Community Development Department, and the City Attorney.
24. Cultural Resources. If a potential Native American resource is uncovered during ground disturbance activities, the Applicant shall halt work in the immediate area of the find, promptly inform the Community Development Department of the suspected presence of that Native American resource, cease earth-disturbing activities in proximity thereto, and retain a qualified professional archaeologist and a culturally-affiliated Native American monitor acceptable to the City for the purpose of examining the potential Native American resource in order to determine whether it is a "tribal cultural resource" as defined in Section 21074(a) of the Public Resources Code (PRC) and/or a "unique cultural resource" as defined in Section 21083.2(g) of the PRC. No additional ground disturbance activities shall occur in the immediate location of the potential Native American resource until all information recovery has been completed, a report filed with the City, and that report made available to interested representatives of Native American tribes that are traditionally and culturally affiliated with the project area.
25. Any future private street/drive shall be subject to City review and approval. Applicant shall be responsible for the design and installation of all required off-site signage related to Irwindale Gateway Specific Plan development.
26. This project shall be subject to Development Impact Fees.

C. FIRE DEPARTMENT

FINAL MAP REQUIREMENTS

FAILURE TO COMPLY WITH THE FINAL MAP REVIEW PRIOR TO RECORDATION, MAY RESULT IN THE BUILDING PLANS NOT BEING APPROVED DURING THE FIRE DEPARTMENT FIRE PREVENTION ENGINEERING SECTION BUILDING PLAN REVIEW.

1. The Final Map shall be submitted to our office for review and approval prior recordation. Access as noted on the Tentative and the Exhibit Maps shall comply with Title 21 (County of Los Angeles Subdivision Code) and Section 503 of the Title 32 (County of Los Angeles Fire Code), which requires an all-weather access surface to be clear to sky.
2. The Private Driveways proposed as private streets for access throughout the development shall be labeled as "Private Driveway" on the Final Map. The portion of the private driveway intended for fire apparatus access shall be identified as "Fire Lane" on the Final Map. All widths and dimensions shall be clearly delineated with a reciprocal access agreement is required for all private driveways. Compliance required prior to Final Map clearance.
3. A common access agreement shall be required for the private driveway since multiple units are sharing the same access. Such language shall be included in the Covenant, Conditions and Restrictions (CC&R) document and shall be submitted to the Fire Department for review prior to Final Map clearance.
4. Fire hydrant improvement plans shall be submitted for review and approval prior final map clearance.

WATER SYSTEM REQUIREMENTS

5. All hydrants shall measure 6"x 4"x 2-1/2" brass or bronze, conforming to current AWWA standard C503 or approved equal and shall be installed in accordance with the County of Los Angeles Fire Code.
6. The required fire flow for the public fire hydrants for this project is 4,000 GPM at 20 psi residual pressure for four (4) hours.
7. All required PUBLIC fire hydrants shall be installed, tested, and accepted prior to beginning construction. Fire Code 501.4.
8. Install 13 new public fire hydrants as Indicated on the Tentative Map.
9. Additional private on-site fire hydrants will be required during the building plan check phase. All required private on-site fire hydrants shall be installed, tested, and approved prior to building occupancy. Fire Code 901.5.1.
10. All on-site fire hydrants shall be installed a minimum of 25' feet from a structure or protected by a two (2) hour rated firewall. Exception: For fully sprinkled multi-family structures, on-site hydrants may be installed a minimum of 10 feet from the

structure. Indicate compliance prior to project proceeding to the public hearing process. Fire Code Appendix C106.1.

11. The required fire flow for the on-site fire hydrants is 2,500 GPM at 20 psi residual pressure for two (2) hours.
12. Vehicular access must be provided and maintained serviceable throughout construction to all required fire hydrants. All required fire hydrants shall be installed, tested, and accepted prior to construction.

D. PUBLIC WORKS/ENGINEERING

STREETS

1. All off-site improvements within the Public Right-of-Way shall be performed in accordance with City Standards to the satisfaction of the City Engineer. Construction plans shall be reviewed and approved by the City Engineer.
2. The owner/developer shall dedicate and construct full width improvements (60 foot right of way, 40' curb to curb) on Live Oak Lane and bear the full cost of design, engineering, installation, construction management and inspection. All improvements shall be constructed in accordance with the City standards to the satisfaction of the City Engineer. Construction plans shall be reviewed and approved by the City Engineer.
3. Adequate "on-site" parking shall be provided per City requirements.
4. The owner/developer shall reimburse the City for the actual cost for the installation, replacement or modification of traffic control signs, striping and pavement markings required in conjunction with the development.
5. The owner/developer shall design and construct ADA accessible driveway approaches, sidewalks, and curb ramps and remove all existing driveways and parkway drains along Live Oak Lane and frontage of Live Oak Avenue. All improvements shall be constructed in accordance with the City standards to the satisfaction of the City Engineer.
6. The owner and/or developer shall remove and reconstruct all damaged or deficient sidewalk, driveways, curb and gutter as directed by the City Engineer.
7. The owner/developer shall be responsible for the repair of all damage to public improvements in the public right-of-way resulting from construction-related activities, including, but not limited to, the movement and/or delivery of equipment, materials and soil to and/or from the site.

8. The owner/developer shall bear the full cost of installing streetlights and/or upgrading the existing streetlights along Live Oak Avenue frontage and Live Oak Lane to more energy-efficient fixtures (LED). Street lighting upgrade shall meet the City of Irwindale and County of Los Angeles Street Lighting Design Standards.
9. Street right-of-way shall be dedicated to the City of Irwindale as follows:
 - a. Two-foot right-of-way along Live Oak Avenue at the project frontage for public sidewalk.
 - b. Right-of-way as required at the intersections of Live Oak Lane and Arrow Highway and Live Oak Lane and Live Oak Avenue sidewalk, intersection and roadway improvements and traffic signals.
 - c. Right-of-way along Live Oak Lane for a public alley fronting APN's 8532-002-020, 8532-002-040, 8532-002-043, 8532-002-036, 8532-002-019, and 8532-002-027.

CITY UTILITIES

10. Storm drains, catch basins, connector pipes, and appurtenances for the site-specific storm drain system shall be designed and constructed in accordance with Los Angeles County standards and the City Engineer's requirements. The owner/developer shall submit grading and drainage plans to the City Engineer for review and approval. The grading and drainage plans shall be prepared by a licensed civil engineer and comply with Los Angeles County grading permit requirements. A hydrology study shall be included with the drainage plan.
11. The owner/developer shall pay for the entire cost for the design, engineering construction and inspection of any upgrade to the water main and connections as required by the serving water company for the proposed development.
12. The owner/developer shall obtain a Storm Drain Connection Permit from the City of Irwindale and/or Los Angeles County Flood Control District for connection(s) to the existing storm drain system.
13. Fire hydrants shall be installed as required by the Fire Department. Existing public fire hydrants adjacent to the site, if any, shall be upgraded if required by the City Engineer and/or Fire Department.
14. Sanitary sewers shall be constructed in accordance with City specifications to serve the subject development. The plans for the sanitary sewers shall be approved by the City Engineer. A sewer study shall be submitted along with the sanitary sewer plans.

- 15. Prior to the issuance of building permits, the owner/developer shall provide a will-serve letter from the water utility provider to the City.

TRAFFIC

- 16. All points of access to the proposed development shall be reviewed and approved by the City Engineer. Project driveways on Live Oak Avenue may be required to allow only right-in and right-out movements. Proper signage shall be installed on the project site.
- 17. Owner/developer shall bear the full cost to design and install a new traffic signal at the intersection of Live Oak Avenue and Live Oak Lane. The improvements shall consist of all necessary traffic signal equipment, signal timing, lane configuration, vehicle detection and striping. Plans shall be reviewed and approved by the City Engineer.

MAPPING

- 18. Final parcel map shall be prepared and submitted to the Public Works Engineering Department for review and approval. The applicant shall be responsible for all fees associated with the review and processing of the final map. The developer shall comply with Los Angeles County's Digital Subdivision Ordinance (DSO) and submit final maps to the City and County in digital format.
- 19. The owner/developer, under the direction of a certified land surveyor and at no cost to the City, shall install all required property boundary monuments, centerline ties and City monuments subject to the City Engineer's approval.
- 20. The owner/developer shall provide, at no cost to the City, one mylar print of the recorded parcel map from the County of Los Angeles Department of Public Works.
- 21. An Easement Agreement for all easements (e.g., roadway, reciprocal access; surface drainage; utilities, etc.) shall be prepared for each parcel, and shall be executed and recorded in the Office of the Los Angeles County Recorder. Such agreements and any CC&Rs shall be subject to the approval of the City Attorney, City Planner and City Engineer.

MISCELLANEOUS

- 22. The owner/developer shall comply with all requirements of the County Sanitation District, make application for and pay the sewer maintenance fee.
- 23. A grading plan shall be submitted for grading/drainage approval to the City Engineer. The owner shall pay grading/drainage review fees in conjunction with this submittal. A professional civil engineer registered in the State of California

shall prepare the grading plans under the current Irwindale adopted Building Codes at the time of submittals.

24. The owner/developer shall comply with the National Pollutant Discharge Elimination System (NPDES) program and shall require the general contractor to implement storm water/urban runoff pollution prevention controls and Best Management Practices (BMPs) on all construction sites in accordance with the City Code. The owner/developer will also be required to submit a Certification for the project and may be required to prepare a Storm Water Pollution Prevention Plan (SWPPP). Projects over five acres in size will be required to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB). The owner/developer can obtain the current application packet by contacting the SWRCB, Construction Storm Water Unit, at (866) 563-3107 or by downloading the forms from their website at: www.waterboards.ca.gov/water_issues/programs/stormwater/construction.shtml. The project shall also conform to the City's Ordinance regarding the requirements for the submittal of a Standard Urban Storm Water Mitigation Plan ("SUSMP"), and the requirements of Low Impact Development ("LID"). The SUSMP includes a requirement to implement Post Construction BMPs to infiltrate the first 3/4" of runoff from all storm events and to control peak-flow discharges.
25. Unless exempted by the Los Angeles Regional Water Quality Control Board, a Covenant and Restriction ensuring the provisions of the approved SWPPP shall also be required.
26. Any changes to the site plan shall be reviewed and approved by the City Engineer.
27. Prior to the approval of the Tract Map the owner/developer shall either construct or post security for all public improvements to be shown on the Tentative Map 83854 and items listed under Public Works/Engineering.
28. Prior to the issuance of a building permit, the project site shall be graded and backfilled to the adjacent street grade elevation, subject to the review and approval of the City Engineer/Building Official.

E. PUBLIC WORKS SERVICES

1. All walls that face the public right-of-way shall implement anti-graffiti management practices (such as planting vines on the walls). Applicant shall be responsible for graffiti removal to all walls that face the public right-of-way in a timely manner.
2. Onsite trash enclosure areas shall have sufficient space for solid waste and organic waste receptacles and shall be covered from the elements with a solid roof structure per Title 14, Division 7, Section 17313 of the California Code of Regulations and Building Code Manual County of Los Angeles Department of Public Works Building and Safety Division 312 Article 2.

3. Tenants must adhere to the SB 1383 regulations set forth by CalRecycle under California Code of Regulations Title 14, Division 7, Chapter 3 and outlined in the Irwindale Municipal Code Section 08.22 Specific Regulations for Organic Waste Disposal Reduction, Recycling, and Solid Waste, which includes the diversion of edible food to food recovery organizations and services.
4. The owner and/or applicant shall remove and reconstruct all damaged, deficient, or substandard sidewalk, driveways, curb, and gutter as directed by the Director of Engineering and the Public Services Director.
5. The owner and/or applicant shall resurface the entire private road/alley and any roadway that will be dedicated public right-of-way (Live Oak Lane) to its entirety and restripe limit line/STOP.
6. The owner and/or applicant is responsible for obtaining encroachment permit(s) from the City prior to commencing any type of work in the public right-of-way.
7. Any work within easements on the project site shall require proper approval and permits from the easement holder (City of Irwindale, County of Los Angeles, Southern California Edison, etc.).
8. Vehicles shall not block the sidewalk or be parked on the public right-of-way along property frontage.
9. All pedestrian and approach ramps in the public right-of-way shall meet ADA requirements. Pedestrian ramps shall have brick red truncated domes in the public right-of-way.
10. The tenant and owner of the property are responsible for the removal of any accidental or illegal dumping of debris on private property. It is not the responsibility of the City of Irwindale to remove any accidental or illegal dumping of items from private property.
11. All utilities and signs within the public right-of-way shall meet horizontal and vertical ADA clearance requirements.
12. Any landscaping, including an irrigation system, in the public right-of-way or any property that is dedicated to the City of Irwindale for maintenance purposes must be approved by the Public Services Director, or designee, as part of the permitting process. Property owner is required to maintain at its sole expense all landscaping in the public right-of-way, including the parkway.